



East Main Street Corridor Improvements  
East Main Street  
MC Project No.: 17005161A  
Appendix

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***EAST MAIN STREET CORRIDOR  
IMPROVEMENTS***

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**APPENDIX A**

**ATTACHMENTS**

**TABLE TSW-1**

**SIGNAL WARRANTS ANALYSIS**

(Based on National Manual of Uniform Traffic Control Devices)

<b>INTERSECTION DATA</b>	
MAJOR STREET: East Main Street	MINOR STREET: Stoney Street
LOCATION: Yorktown, NY	
DATE: 4/11/18	
VOLUME BASIS..... Existing Traffic Volumes	
CONDITION ..... Typical Weekday	

<b>CHARACTERISTICS</b>	
<b>Number Of Lanes For Moving Traffic By Approach</b>	
Major Street (Excluding Auxiliary Lanes) =	1
Minor Street (Including Auxiliary Lanes) =	1
<b>Speed</b>	
85 % Speed >= 40 mph (Y or N)----->	N
<b>Population</b>	
Community < 10,000 (Y or N)----->	N

TIME	VOLUMES		WARRANT 1 CONDITION A		WARRANT 1 CONDITION B		WARRANT 1 CONDITION A & B COMBINED				WARRANT MET?			
							CONDITION A		CONDITION B		1A	1B	COMBINED	
							Major Street	Minor Street	Major Street	Minor Street			1A	1B
Hour Begin	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	1A	1B
12:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
01:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
02:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
03:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
04:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
05:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
06:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
07:00 AM	796	101	500	150	750	75	400	120	600	60	NO	YES	NO	YES
08:00 AM	797	112	500	150	750	75	400	120	600	60	NO	YES	NO	YES
09:00 AM	651	88	500	150	750	75	400	120	600	60	NO	NO	NO	YES
10:00 AM	507	97	500	150	750	75	400	120	600	60	NO	NO	NO	NO
11:00 AM	546	100	500	150	750	75	400	120	600	60	NO	NO	NO	NO
12:00 PM	524	116	500	150	750	75	400	120	600	60	NO	NO	NO	NO
01:00 PM	611	110	500	150	750	75	400	120	600	60	NO	NO	NO	YES
02:00 PM	729	121	500	150	750	75	400	120	600	60	NO	NO	YES	YES
03:00 PM	758	146	500	150	750	75	400	120	600	60	NO	YES	YES	YES
04:00 PM	902	177	500	150	750	75	400	120	600	60	YES	YES	YES	YES
05:00 PM	913	199	500	150	750	75	400	120	600	60	YES	YES	YES	YES
06:00 PM	747	147	500	150	750	75	400	120	600	60	NO	NO	YES	YES
07:00 PM	613	98	500	150	750	75	400	120	600	60	NO	NO	NO	YES
08:00 PM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
09:00 PM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
10:00 PM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO

<b>TOTAL HOURS MEETING WARRANTS</b>	<b>2</b>	<b>5</b>	<b>5</b>	<b>10</b>
<b>TOTAL HOURS NEEDED TO SATISFY</b>	<b>8</b>	<b>8</b>	<b>8*</b>	<b>8*</b>

MINIMUM VEHICULAR VOLUME	WARRANT 1A: NOT SATISFIED -- NO SIGNAL
INTERRUPTION OF CONTINUOUS TRAFFIC	WARRANT 1B: NOT SATISFIED -- NO SIGNAL
COMBINED CONDITION	WARRANT 1A & 1B COMBINED: NOT SATISFIED -- NO SIGNAL
*NOTE: FOR COMBINED WARRANT BOTH CONDCTIONS 1A & 1B MUST BE SATISFIED FOR 8 HOURS.	

**TABLE TSW-2**

**SIGNAL WARRANTS ANALYSIS**

(Based on National Manual of Uniform Traffic Control Devices)

<b>INTERSECTION DATA</b>	
MAJOR STREET:	East Main Street
MINOR STREET:	Stoney Street
LOCATION:	Yorktown, NY
DATE:	4/11/18
VOLUME BASIS.....	Existing Traffic Volumes
CONDITION .....	Typical Weekday

<b>CHARACTERISTICS</b>	
<b>Number Of Lanes For Moving Traffic By Approach</b>	
Major Street (Excluding Auxiliary Lanes) =	1
Minor Street (Including Auxiliary Lanes) =	1
<b>Speed</b>	
85 % Speed >= 40 mph (Y or N)----->	N
<b>Median</b>	
Raised median 4' or more in width on major street (Y or N)?----->	N
<b>Population</b>	
Community < 10,000 (Y or N)----->	N

TIME	VOLUMES		WARRANT 2 <sup>1</sup>		WARRANT 3 <sup>1</sup>		WARRANT MET?		
	Hour Begin	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	2	3
12:00 AM		0	0					NO	NO
01:00 AM		0	0					NO	NO
02:00 AM		0	0					NO	NO
03:00 AM		0	0					NO	NO
04:00 AM		0	0					NO	NO
05:00 AM		0	0					NO	NO
06:00 AM		0	0					NO	NO
07:00 AM		796	101					NO	NO
08:00 AM		797	112					NO	NO
09:00 AM		651	88					NO	NO
10:00 AM		507	97					NO	NO
11:00 AM		546	100					NO	NO
12:00 PM		524	116					NO	NO
01:00 PM		611	110					NO	NO
02:00 PM		729	121					NO	NO
03:00 PM		758	146					NO	NO
04:00 PM		902	177					YES	NO
05:00 PM		913	199					YES	NO
06:00 PM		747	147					NO	NO
07:00 PM		613	98					NO	NO
08:00 PM		0	0					NO	NO
09:00 PM		0	0					NO	NO
10:00 PM		0	0					NO	NO

NOTE major peds = highest volume on major street crosswalk

<b>TOTAL HOURS MEETING WARRANTS</b>	<b>2</b>	<b>0</b>
<b>TOTAL HOURS NEEDED TO SATISFY</b>	<b>4</b>	<b>1</b>

FOUR HOUR VEHICULAR VOLUME	WARRANT 2: NOT SATISFIED -- NO SIGNAL
PEAK HOUR VOLUME	WARRANT 3: NOT SATISFIED -- NO SIGNAL

**NOTES:**

1) VOLUMES FOR WARRANTS 2 AND 3 ARE COMPARED TO MUTCD FIGURE 4C-2 FOR WARRANT 2 AND FIGURE 4C-4 FOR WARRANT 3 ATTACHED.

**TABLE TSW-B1**

**SIGNAL WARRANTS ANALYSIS**

(Based on National Manual of Uniform Traffic Control Devices)

<b>INTERSECTION DATA</b>	
MAJOR STREET:	East Main Street
MINOR STREET:	Stoney Street
LOCATION:	Yorktown, NY
DATE:	4/11/18
VOLUME BASIS.....	Build Traffic Volumes
CONDITION .....	Typical Weekday

<b>CHARACTERISTICS</b>	
<b>Number Of Lanes For Moving Traffic By Approach</b>	
Major Street (Excluding Auxiliary Lanes) =	1
Minor Street (Including Auxiliary Lanes) =	1
<b>Speed</b>	
85 % Speed >= 40 mph (Y or N)----->	N
<b>Population</b>	
Community < 10,000 (Y or N)----->	N

TIME	VOLUMES		WARRANT 1 CONDITION A		WARRANT 1 CONDITION B		WARRANT 1 CONDITION A & B COMBINED				WARRANT MET?			
							CONDITION A		CONDITION B		1A	1B	COMBINED	
							Major Street	Minor Street	Major Street	Minor Street			1A	1B
Hour Begin	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	1A	1B
12:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
01:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
02:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
03:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
04:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
05:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
06:00 AM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
07:00 AM	875	126	500	150	750	75	400	120	600	60	NO	YES	YES	YES
08:00 AM	968	154	500	150	750	75	400	120	600	60	YES	YES	YES	YES
09:00 AM	716	121	500	150	750	75	400	120	600	60	NO	NO	YES	YES
10:00 AM	557	126	500	150	750	75	400	120	600	60	NO	NO	YES	NO
11:00 AM	600	110	500	150	750	75	400	120	600	60	NO	NO	NO	YES
12:00 PM	576	127	500	150	750	75	400	120	600	60	NO	NO	YES	NO
01:00 PM	672	121	500	150	750	75	400	120	600	60	NO	NO	YES	YES
02:00 PM	801	133	500	150	750	75	400	120	600	60	NO	YES	YES	YES
03:00 PM	833	160	500	150	750	75	400	120	600	60	YES	YES	YES	YES
04:00 PM	992	194	500	150	750	75	400	120	600	60	YES	YES	YES	YES
05:00 PM	1008	286	500	150	750	75	400	120	600	60	YES	YES	YES	YES
06:00 PM	821	161	500	150	750	75	400	120	600	60	YES	YES	YES	YES
07:00 PM	674	107	500	150	750	75	400	120	600	60	NO	NO	NO	YES
08:00 PM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
09:00 PM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO
10:00 PM	0	0	500	150	750	75	400	120	600	60	NO	NO	NO	NO

<b>TOTAL HOURS MEETING WARRANTS</b>	<b>5</b>	<b>7</b>	<b>11</b>	<b>11</b>
<b>TOTAL HOURS NEEDED TO SATISFY</b>	<b>8</b>	<b>8</b>	<b>8*</b>	<b>8*</b>

MINIMUM VEHICULAR VOLUME	WARRANT 1A: NOT SATISFIED -- NO SIGNAL
INTERRUPTION OF CONTINUOUS TRAFFIC	WARRANT 1B: NOT SATISFIED -- NO SIGNAL
COMBINED CONDITION	WARRANT 1A & 1B COMBINED: SATISFIED -- CRITERIA MET FOR SIGNALIZATION
*NOTE: FOR COMBINED WARRANT BOTH CONDCTIONS 1A & 1B MUST BE SATISFIED FOR 8 HOURS.	

**TABLE TSW-B2**

**SIGNAL WARRANTS ANALYSIS**

(Based on National Manual of Uniform Traffic Control Devices)

<b>INTERSECTION DATA</b>	
MAJOR STREET:	East Main Street
MINOR STREET:	Stoney Street
LOCATION:	Yorktown, NY
DATE:	4/11/18
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CONDITION .....	Typical Weekday

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<b>Speed</b>	
85 % Speed >= 40 mph (Y or N)----->	N
<b>Median</b>	
Raised median 4' or more in width on major street (Y or N)?----->	N
<b>Population</b>	
Community < 10,000 (Y or N)----->	N

TIME	VOLUMES		WARRANT 2 <sup>1</sup>		WARRANT 3 <sup>1</sup>		WARRANT MET?		
	Hour Begin	Major Street	Minor Street	Major Street	Minor Street	Major Street	Minor Street	2	3
12:00 AM		0	0	SEE FIGURE 4C-1	SEE FIGURE 4C-3			NO	NO
01:00 AM		0	0					NO	NO
02:00 AM		0	0					NO	NO
03:00 AM		0	0					NO	NO
04:00 AM		0	0					NO	NO
05:00 AM		0	0					NO	NO
06:00 AM		0	0					NO	NO
07:00 AM		875	126					NO	NO
08:00 AM		968	154					YES	NO
09:00 AM		716	121					NO	NO
10:00 AM		557	126					NO	NO
11:00 AM		600	110					NO	NO
12:00 PM		576	127					NO	NO
01:00 PM		672	121					NO	NO
02:00 PM		801	133					NO	NO
03:00 PM		833	160					YES	NO
04:00 PM		992	194					YES	NO
05:00 PM		1008	286					YES	YES
06:00 PM		821	161					YES	NO
07:00 PM		674	107					NO	NO
08:00 PM		0	0					NO	NO
09:00 PM		0	0					NO	NO
10:00 PM		0	0					NO	NO

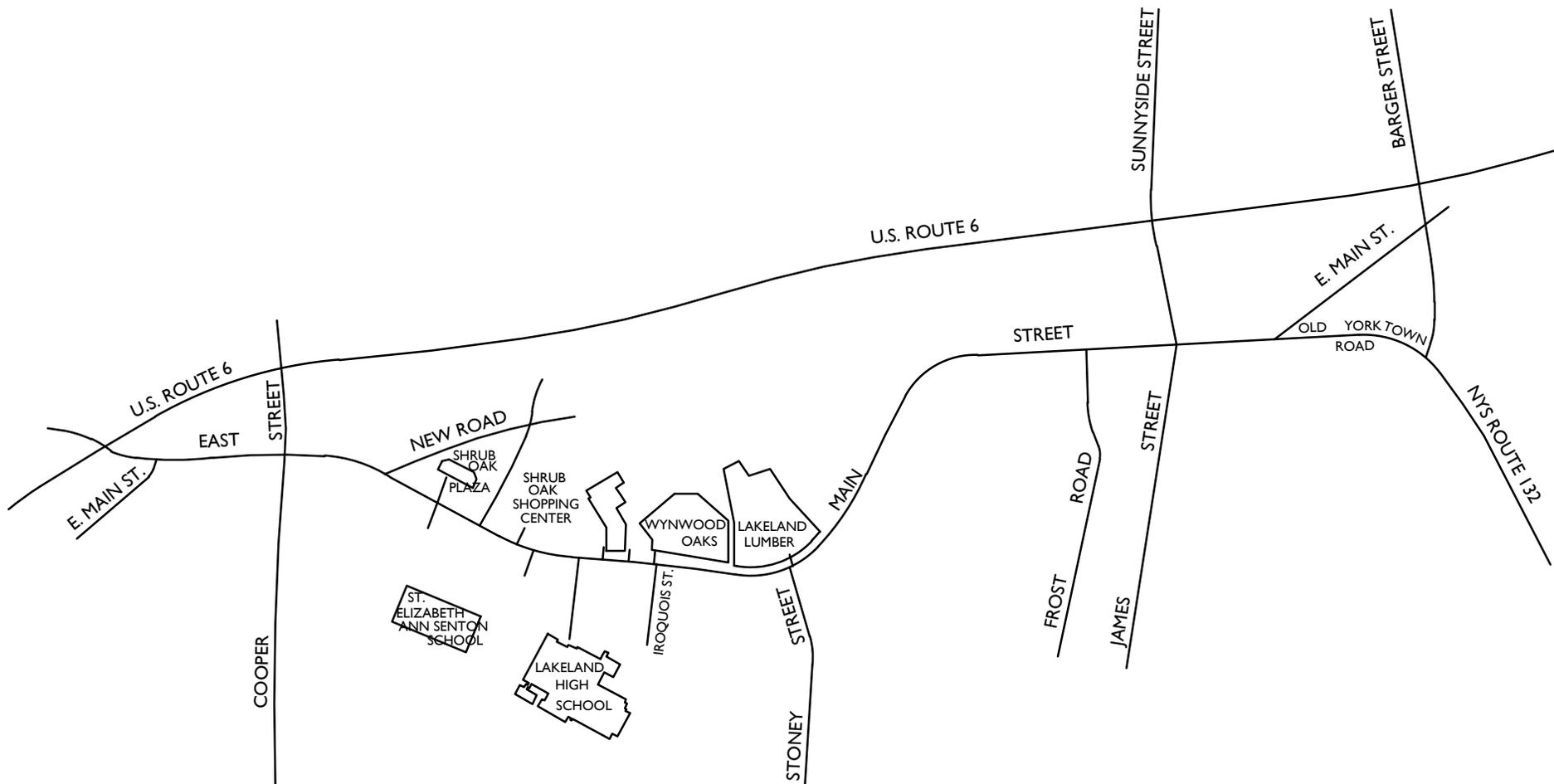
NOTE major peds = highest volume on major street crosswalk

<b>TOTAL HOURS MEETING WARRANTS</b>	<b>5</b>	<b>1</b>
<b>TOTAL HOURS NEEDED TO SATISFY</b>	<b>4</b>	<b>1</b>

FOUR HOUR VEHICULAR VOLUME	WARRANT 2: SATISFIED -- CRITERIA MET FOR SIGNALIZATION
PEAK HOUR VOLUME	WARRANT 3: SATISFIED -- CRITERIA MET FOR SIGNALIZATION

**NOTES:**

1) VOLUMES FOR WARRANTS 2 AND 3 ARE COMPARED TO MUTCD FIGURE 4C-2 FOR WARRANT 2 AND FIGURE 4C-4 FOR WARRANT 3 ATTACHED.



NOTE: VOLUMES OBTAINED FROM TRAFFIC IMPACT ANALYSIS FOR SHRUB OAK INTERNATIONAL SCHOOL PREPARED BY H2M ARCHITECTS + ENGINEERS

NOTE: LINE DIAGRAM NOT TO SCALE



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- Pittsburgh, PA
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- Orlando, FL
- Miami, FL
- Sterling, VA
- Norfolk, VA
- Albuquerque, NM
- Charlotte, NC

REV	DATE	DRAWN BY	DESCRIPTION

**EAST MAIN STREET  
CORRIDOR**

EAST MAIN STREET  
YORKTOWN  
WESTCHESTER COUNTY  
NEW YORK



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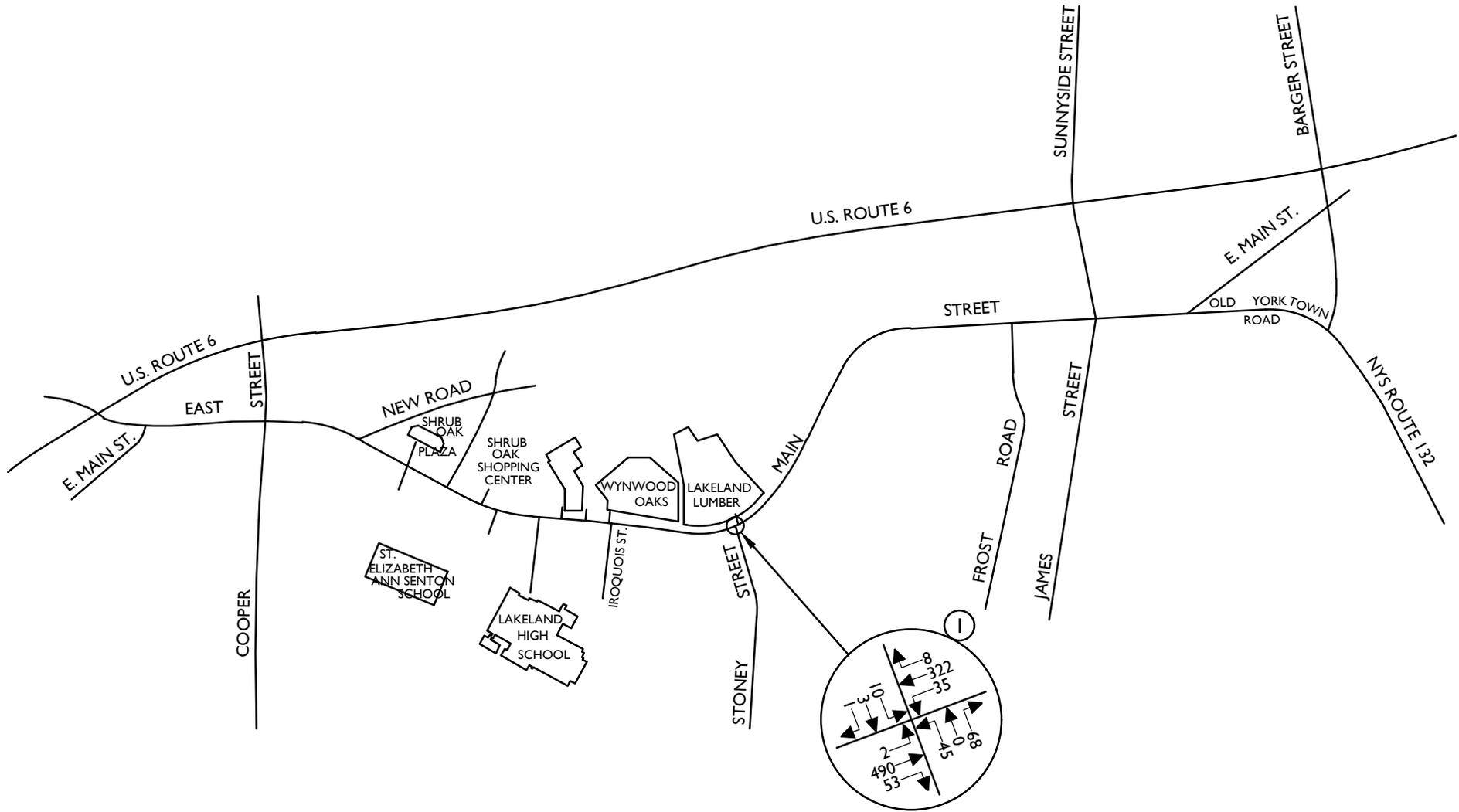
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Valhalla, NY 10595  
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Fax: 914.347.7266

TRAFFIC IMPACT STUDY

SCALE	DATE	DRAWN BY	CHECKED BY
AS SHOWN	4/10/18	R.H.	P.J.G.
PROJECT NUMBER	DRAWING NAME		
17005162A	180410RH_FIGURE		

SHEET TITLE:  
SITE LOCATION MAP

SHEET NUMBER:  
FIG. NO. 1



NOTE: VOLUMES OBTAINED FROM TRAFFIC IMPACT ANALYSIS FOR SHRUB OAK INTERNATIONAL SCHOOL PREPARED BY H2M ARCHITECTS + ENGINEERS

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- Columbia, MD
- Lehigh Valley, PA
- Exton, PA
- Philadelphia, PA
- Pittsburgh, PA
- Tampa, FL
- Orlando, FL
- Miami, FL
- Sterling, VA
- Norfolk, VA
- Albuquerque, NM
- Charlotte, NC

REV	DATE	DRAWN BY	DESCRIPTION

EAST MAIN STREET CORRIDOR

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WESTCHESTER COUNTY  
NEW YORK



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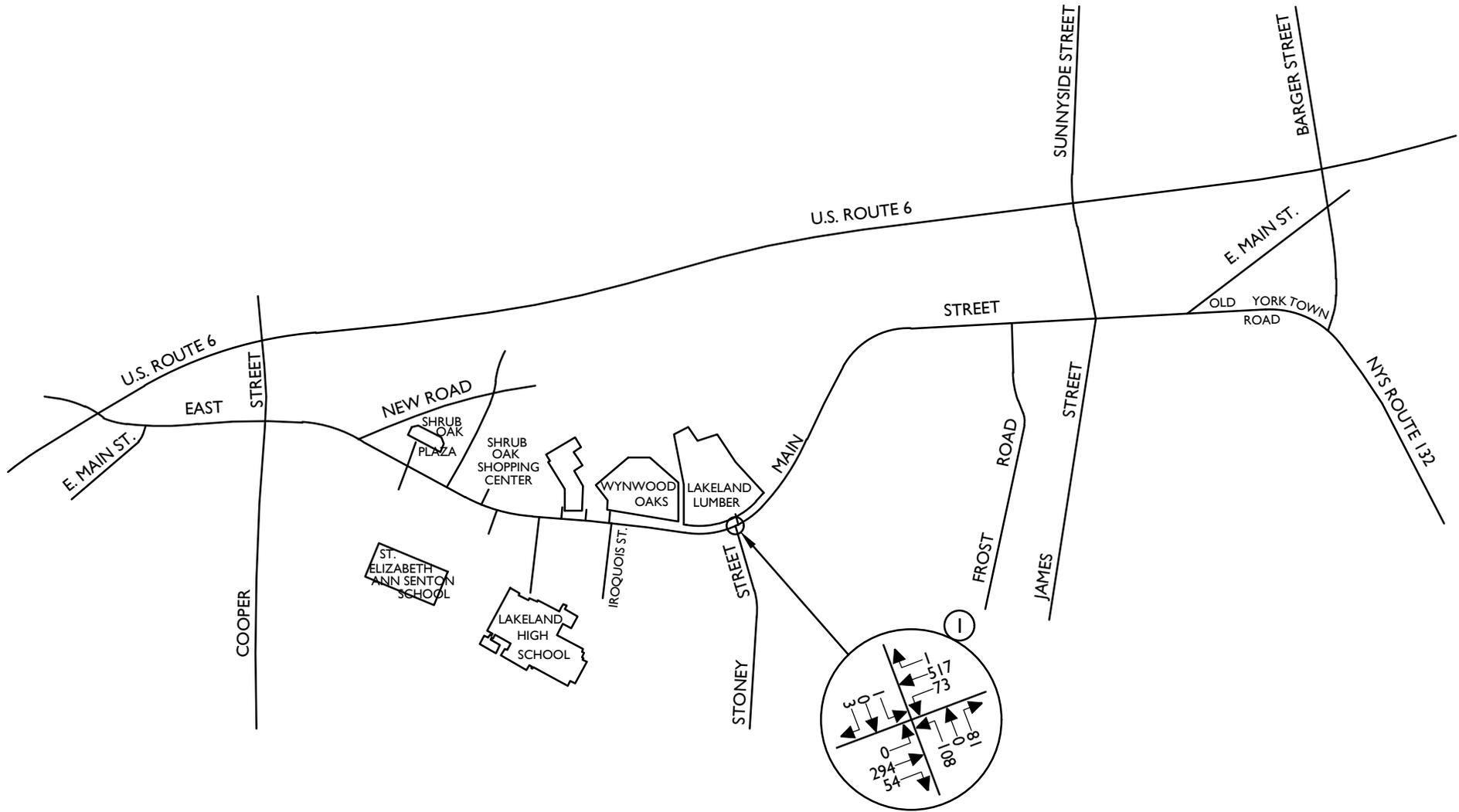
TRAFFIC IMPACT STUDY

SCALE	DATE	DRAWN BY	CHECKED BY
AS SHOWN	4/10/18	R.H.	P.J.G.

PROJECT NUMBER	DRAWING NAME
17005162A	180410RH_FIGURE

SHEET TITLE:  
**2017 EXISTING TRAFFIC VOLUMES  
WEEKDAY PEAK AM HOUR**

SHEET NUMBER:  
**FIG. NO. 2**



NOTE: VOLUMES OBTAINED FROM TRAFFIC IMPACT ANALYSIS FOR SHRUB OAK INTERNATIONAL SCHOOL PREPARED BY H2M ARCHITECTS + ENGINEERS

NOTE: LINE DIAGRAM NOT TO SCALE



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- Miami, FL
- Sterling, VA
- Norfolk, VA
- Albuquerque, NM
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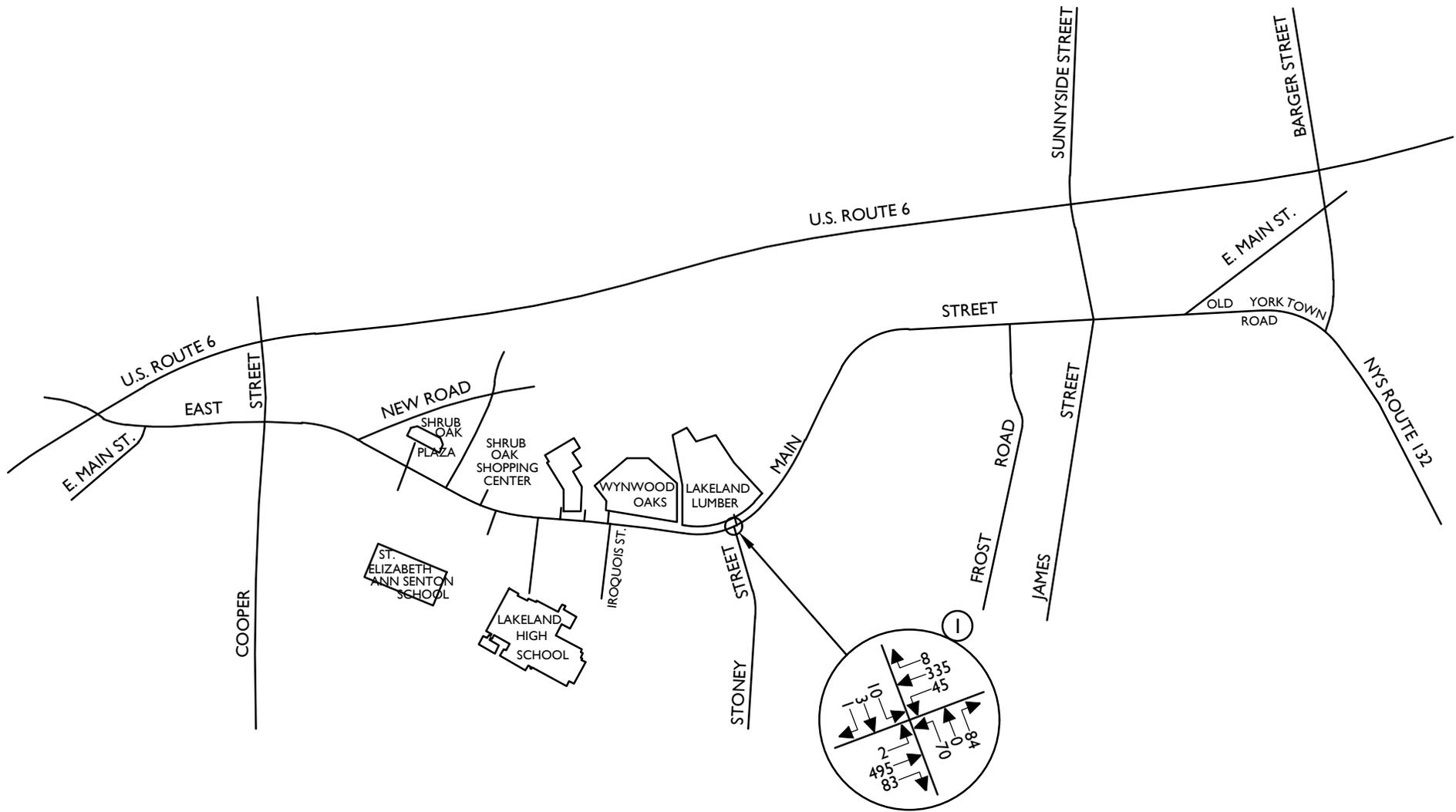
Phone: 914.347.7500  
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TRAFFIC IMPACT STUDY

SCALE	DATE	DRAWN BY	CHECKED BY
AS SHOWN	4/10/18	R.H.	P.J.G.
PROJECT NUMBER	DRAWING NAME		
17005162A	180410RH_FIGURE		

SHEET TITLE:  
**2017 EXISTING TRAFFIC VOLUMES  
WEEKDAY PEAK PM HOUR**

SHEET NUMBER:  
**FIG. NO. 3**



NOTE: VOLUMES OBTAINED FROM TRAFFIC IMPACT ANALYSIS FOR SHRUB OAK INTERNATIONAL SCHOOL PREPARED BY H2M ARCHITECTS + ENGINEERS

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- Philadelphia, PA
- Pittsburgh, PA
- Tampa, FL
- Orlando, FL
- Miami, FL
- Sterling, VA
- Norfolk, VA
- Albuquerque, NM
- Charlotte, NC

REV	DATE	DRAWN BY	DESCRIPTION

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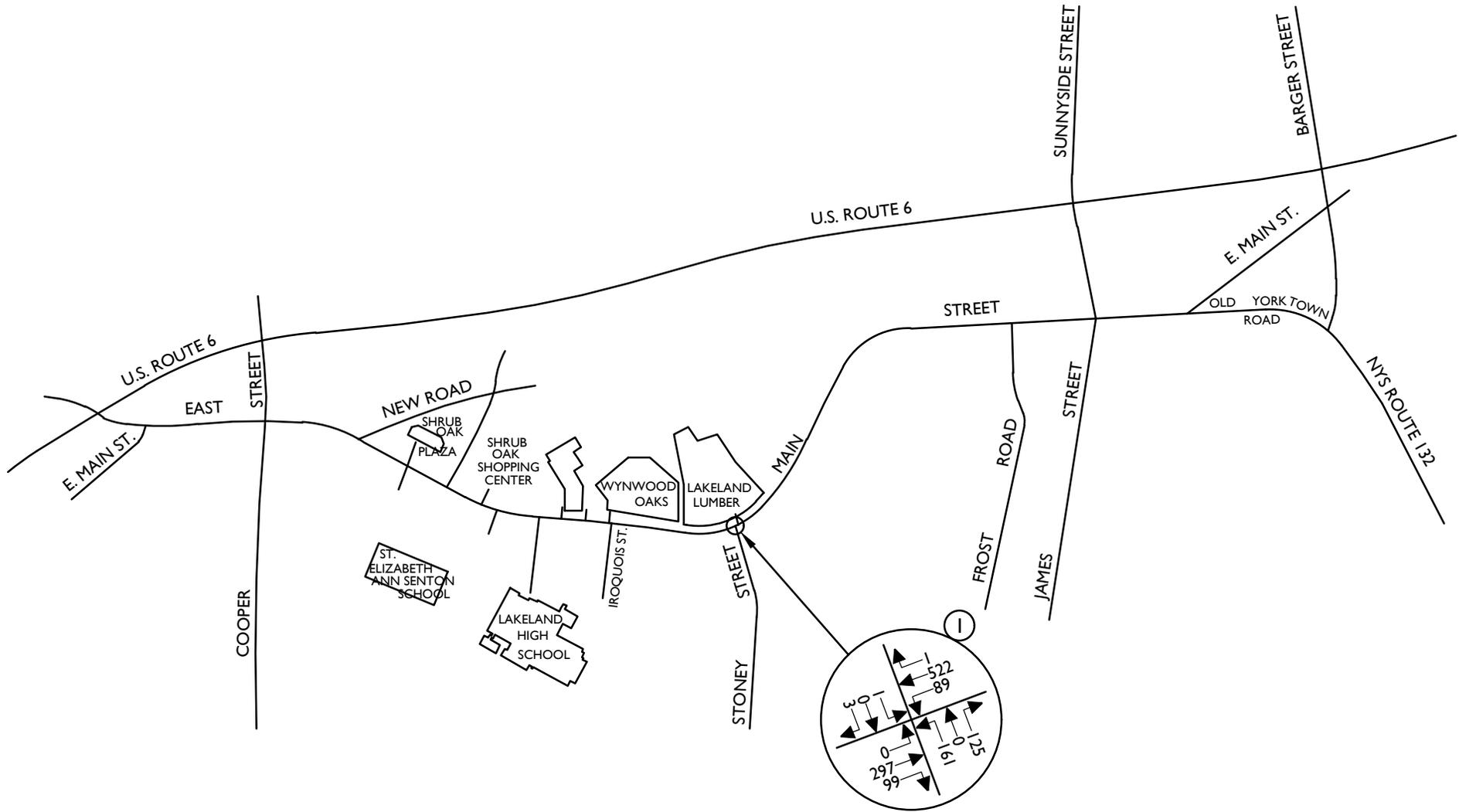
Phone: 914.347.7500  
Fax: 914.347.7266

TRAFFIC IMPACT STUDY

SCALE	DATE	DRAWN BY	CHECKED BY
AS SHOWN	4/10/18	R.H.	P.J.G.
PROJECT NUMBER	DRAWING NAME		
17005162A	180410RH_FIGURE		

SHEET TITLE:  
**2018 BUILD TRAFFIC VOLUMES  
WEEKDAY PEAK AM HOUR**

SHEET NUMBER:  
**FIG. NO. 4**



NOTE: VOLUMES OBTAINED FROM TRAFFIC IMPACT ANALYSIS FOR SHRUB OAK INTERNATIONAL SCHOOL PREPARED BY H2M ARCHITECTS + ENGINEERS

NOTE: LINE DIAGRAM NOT TO SCALE

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REV	DATE	DRAWN BY	DESCRIPTION

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CORRIDOR**

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YORKTOWN  
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TRAFFIC IMPACT STUDY			
SCALE	DATE	DRAWN BY	CHECKED BY
AS SHOWN	4/10/18	R.H.	P.J.G.
PROJECT NUMBER	DRAWING NAME		
17005162A	180410RH_FIGURE		
SHEET TITLE:			
2018 BUILD TRAFFIC VOLUMES WEEKDAY PEAK PM HOUR			
SHEET NUMBER:			
FIG. NO. 5			

# TABLE V-1

## HOURLY VOLUME SUMMARY (WEEKDAY AVERAGE VOLUMES)

JOB #                    17005162A  
 LOCATION:            YORKTOWN, NY  
 DAY:                    WEEKDAY  
 TIME:                   7:00 AM - 8:00 PM

TIME	EAST MAIN STREET			STONEY STREET
	EASTBOUND	WESTBOUND	TOTAL	NORTHBOUND
7:00 AM 8:00 AM	632	164	796	101
8:00 AM 9:00 AM	566	231	797	112
9:00 AM 10:00 AM	369	282	651	88
10:00 AM 11:00 AM	276	231	507	97
11:00 AM 12:00 PM	272	274	546	100
12:00 PM 1:00 PM	260	264	524	116
1:00 PM 2:00 PM	315	296	611	110
2:00 PM 3:00 PM	330	399	729	121
3:00 PM 4:00 PM	310	448	758	146
4:00 PM 5:00 PM	347	555	902	177
5:00 PM 6:00 PM	347	566	913	199
6:00 PM 7:00 PM	321	426	747	147
7:00 PM 8:00 PM	259	354	613	98
<b>TOTALS</b>	<b>4604</b>	<b>4490</b>	<b>9094</b>	<b>1612</b>

(1) HOURLY APPROACH VOLUMES OF THE INTERSECTION OF EAST MAIN STREET & STONEY STREET FROM THE H2M REPORT DATED APRIL 2017.

TABLE NO. V-2

SUMMARY OF EXISTING ADDITIONAL INTERSECTION AND ROADWAY SEGMENT FEATURES (1)

ID	LOCATION	SIGNALIZATION	SIDEWALK & WIDTHS	PEDESTRIAN CROSS WALK	APPROXIMATE SHOULDER WIDTH	APPROACH / LANES & WIDTH	CENTERLINE MARKING	OTHER LANE MARKINGS	PARKING REGULATIONS	SPEED LIMIT	GENERAL ALIGNMENT	APPRX. GRADES	NOTES	
														LTR
1	EAST MAIN STREET & STONEY STREET/LAKELAND LUMBER	UNSIGNALIZED	EB	YES	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	NORTHEAST	-4%	
			WB	YES	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	SOUTHWEST	0%	
			NB	NO	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	NORTHWEST	-4%	
			SB	NO	NO	NONE	15'	NONE	NONE	NO PARKING	30 MPH	SOUTHEAST	0%	
2	EAST MAIN STREET & IROQUOIS STREET/WYNWOOD OAKS	UNSIGNALIZED	EB	YES	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	STRAIGHT	0%	
			WB	YES	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	STRAIGHT	-3%	
			NB	LEFT SIDE YES RIGHT CORNER ONLY (2)	NO	NONE	10'	SWSL	-	NO PARKING	30 MPH	STRAIGHT	-3%	
			SB	YES	NO	NONE	14'	SWL	-	NO PARKING	30 MPH	STRAIGHT	-1%	
3	EAST MAIN STREET & SHRUB OAK SHOPPING CENTER (ENTRY DRIVEWAY)	UNSIGNALIZED	EB	RIGHT SIDE YES LEFT SIDE YES	NO	NONE	11'	DYCL	-	NO PARKING	30 MPH	SOUTHEAST	2%	
			WB	RIGHT CORNER ONLY (2)	YES	NONE	11'	DYCL	-	NO PARKING	30 MPH	NORTHWEST	1%	
4	EAST MAIN STREET & LAKELAND HIGH SCHOOL DRIVEWAY	UNSIGNALIZED	EB	RIGHT SIDE YES	NO	NONE	11'	DYCL	-	NO PARKING	30 MPH	SOUTHEAST	2%	
			WB	LEFT SIDE YES	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	NORTHWEST	0%	
			NB	LEFT CORNER ONLY (2) RIGHT SIDE YES	NO	NONE	15'	SWSL	-	NO PARKING	30 MPH	NORTHEAST	-2%	
5	EAST MAIN STREET & ST. ELIZABETH ANN SENTON SCHOOL	UNSIGNALIZED	EB	RIGHT SIDE YES	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	SOUTHEAST	2%	
			WB	LEFT SIDE YES	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	NORTHWEST	-2%	
			NB	LEFT & RIGHT CORNER (2)	NO	NONE	11'	NONE	NONE	NO PARKING	30 MPH	NORTHEAST	-3%	
6	EAST MAIN STREET & SHRUB OAK SHOPPING CENTER (EXIT DRIVEWAY)	UNSIGNALIZED	EB	RIGHT SIDE YES	NO	NONE	11'	DYCL	-	NO PARKING	30 MPH	SOUTHEAST	2%	
			WB	LEFT SIDE YES	NO	NONE	12'	DYCL	-	NO PARKING	30 MPH	NORTHWEST	-3%	
			SB	NO	NO	NONE	24'	NONE	NONE	NO PARKING	30 MPH	SOUTHWEST	3%	

NOTES:

- 1) SEE FIGURE NO. 1A FOR LANE GEOMETRY.
- 2) SMALL LANDING AREA FOR PEDESTRIANS ON NORTHEAST, SOUTHEAST, AND SOUTH WEST CORNERS ONLY
- 3) DYCL - DOUBLE YELLOW CENTER LINE, BWL - BROKEN WHITE LINE, SWL - SOLID WHITE LINE, SWSL - SOLID WHITE STOP LINE

**TABLE V-3**

**EAST MAIN STREET PEDESTRIAN CROSSING SUMMARY TABLE**

	AT SHRUB OAK SHOPPING CENTER EAST DRIVEWAY	AT ALGONQUIN STREET EAST CORNER	AT ALGONQUIN STREET WEST CORNER	AT SHRUB OAK SHOPPING CENTER WEST DRIVEWAY
EAST MAIN STREET	13	0	2	35

NOTES:

1) THE ABOVE REPRESENTS THE PEDESTRIANS CROSSING EAST MAIN STREET AT 4 LOCATIONS. DURING THE PERIOD FROM 2:00 PM TO 3:00 PM AS OBSERVED ON APRIL 4, 2018.

**TABLE NO. LOS-1**  
**LEVEL OF SERVICE SUMMARY TABLE**

				2017 EXISTING		2018 NO-BUILD		2018 BUILD	
				AM	PM	AM	PM	AM	PM
1	E. MAIN STREET & STONEY STREET/DRIVEWAY	<b>UN SIGNALIZED</b>							
		EB	LTR	A[8.2]	A[0.0]	A[8.2]	A[0.0]	A[8.2]	A[0.0]
		WB	LTR	A[8.7]	A[8.3]	A[8.9]	A[8.4]	A[8.9]	A[8.5]
		NB	LTR	C[18.9]	E[41.4]	C[24.5]	F[106.0]	D[25.5]	F[145.2]
		SB	LTR	D[26.7]	C[18.3]	D[30.3]	C[19.7]	D[30.9]	C[20.5]
		<u>W/ SIGNALIZATION</u>							
		EB	LTR	-	-	A[4.9]	A[4.8]	A[5.1]	A[5.4]
		WB	LTR	-	-	A[4.5]	A[7.2]	A[4.6]	A[8.5]
		NB	LTR	-	-	B[10.4]	B[16.0]	B[10.4]	B[16.9]
		SB	LTR	-	-	A[9.2]	B[12.7]	A[9.2]	B[12.8]
		<b>OVERALL</b>		-	-	<b>A[5.5]</b>	<b>A[8.1]</b>	<b>A[5.6]</b>	<b>A[9.4]</b>

NOTES:

1) THE ABOVE REPRESENTS THE LEVEL OF SERVICE AND VEHICLE DELAY IN SECONDS, C [16.2], FOR EACH KEY APPROACH OF THE UNSIGNALIZED INTERSECTIONS AS WELL AS FOR EACH APPROACH AND THE OVERALL INTERSECTION FOR THE SIGNALIZED INTERSECTIONS.

2) CONDITIONS WITH TRAFFIC SIGNAL INSTALLATION.

3) SEE APPENDIX "C" FOR A DESCRIPTION OF THE LEVELS OF SERVICE.







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PROJECT NUMBER:	DRAWING NAME:		
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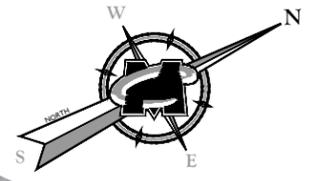
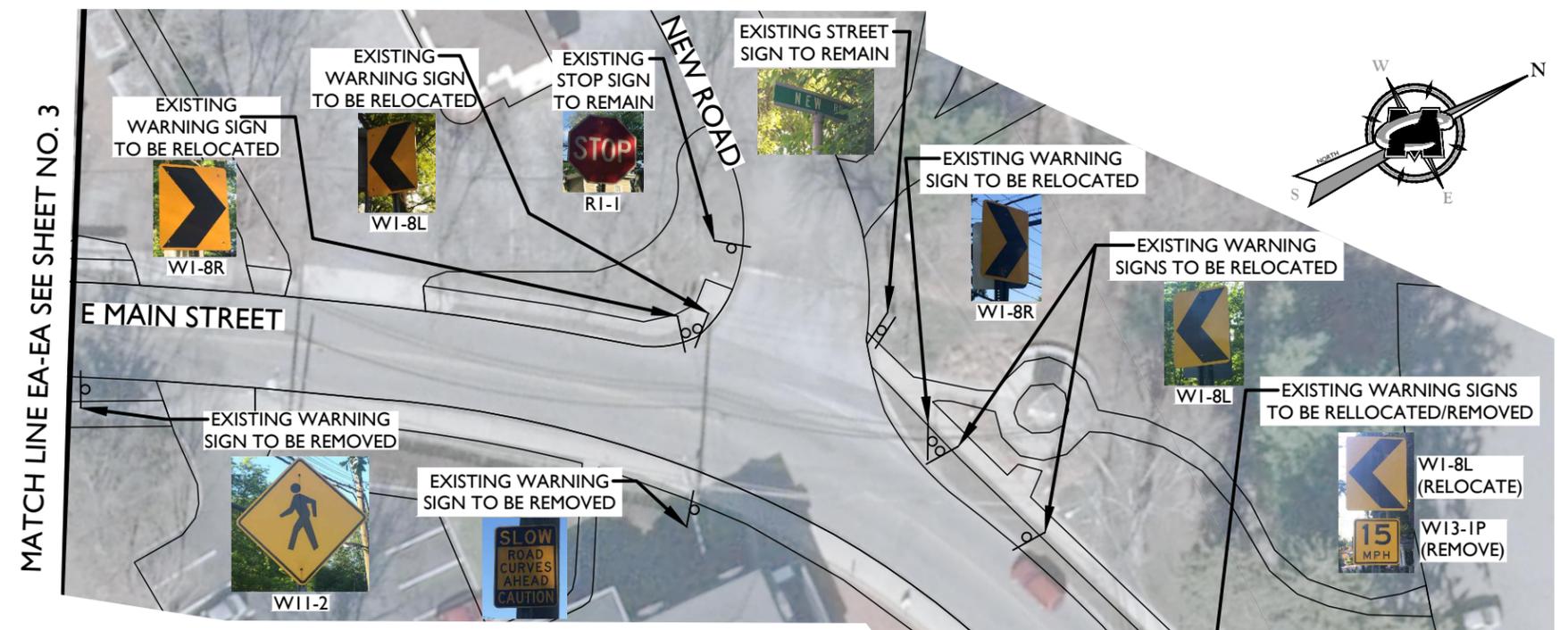
SHEET TITLE:  
**KEY SHEET**

SHEET NUMBER:  
 1 of 10

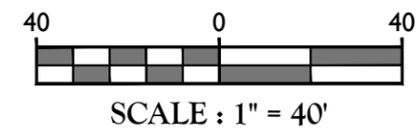
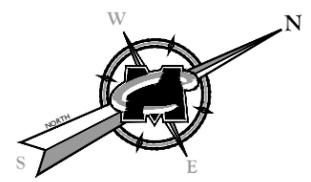
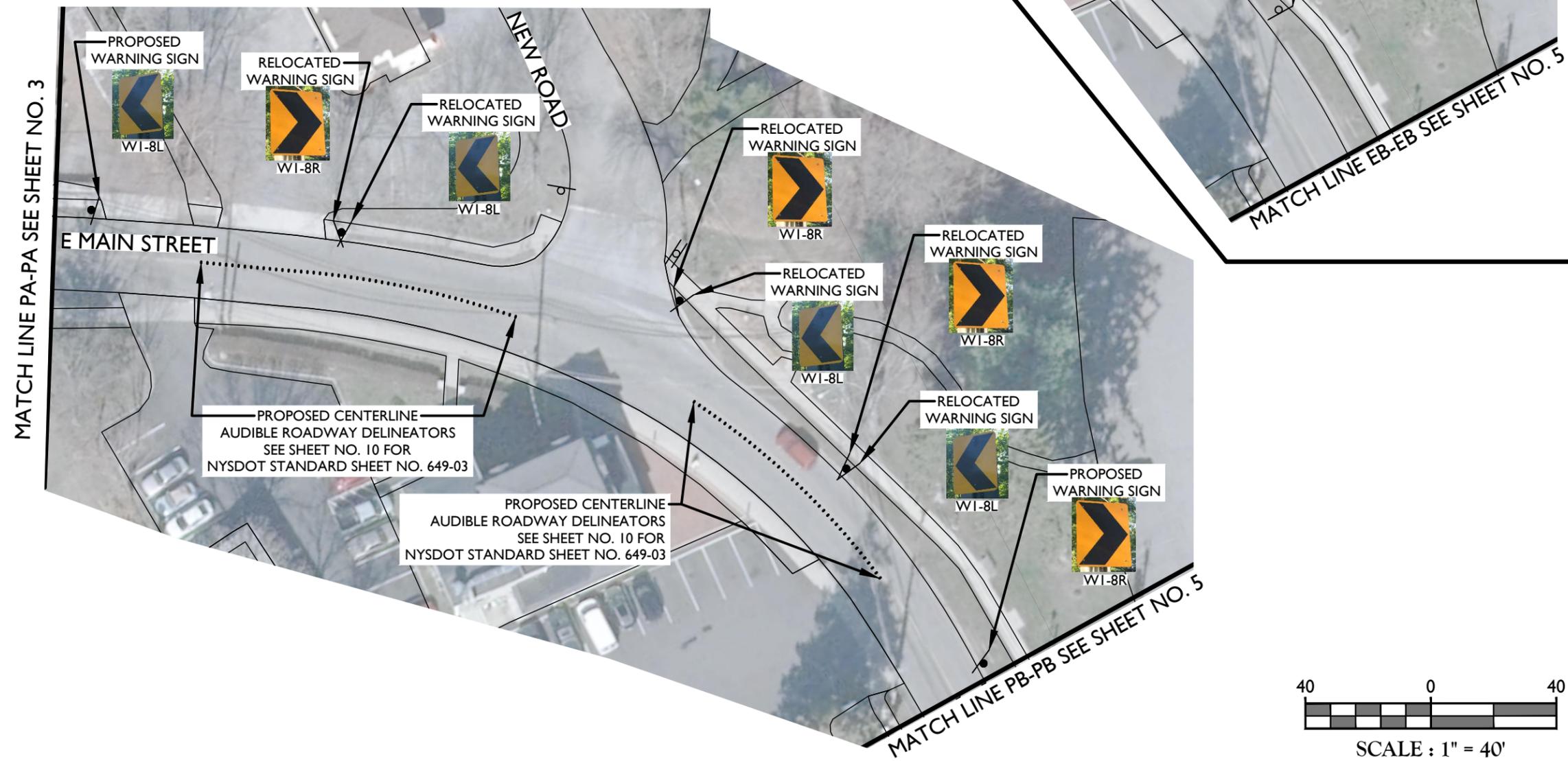




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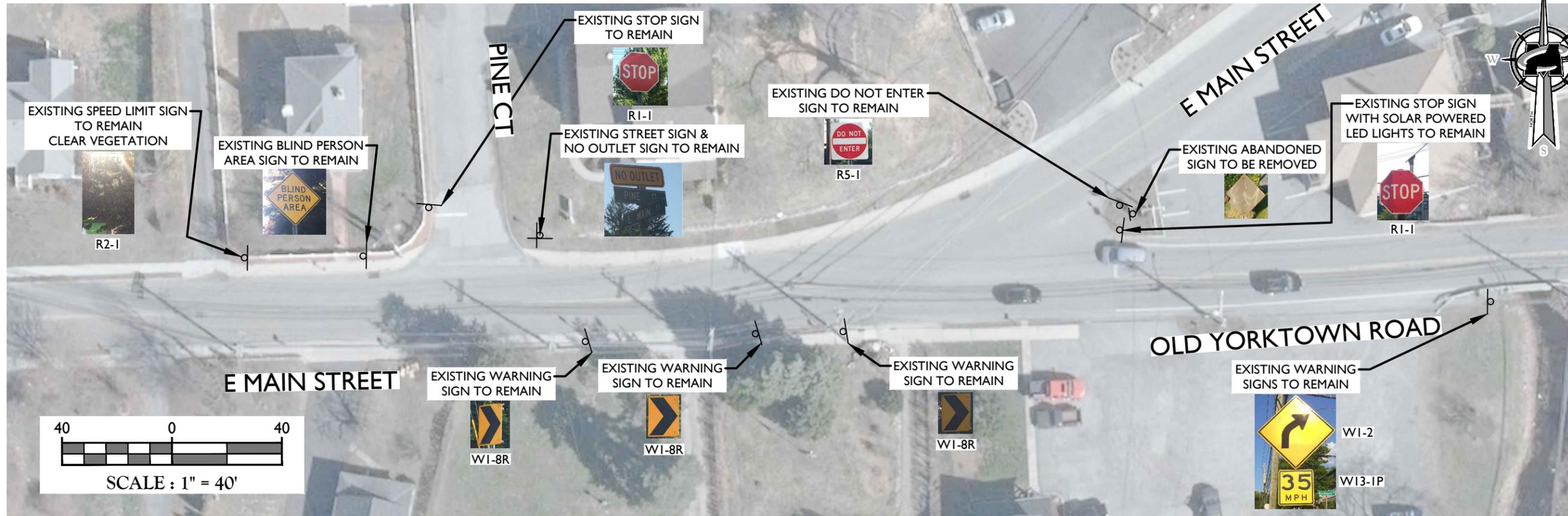
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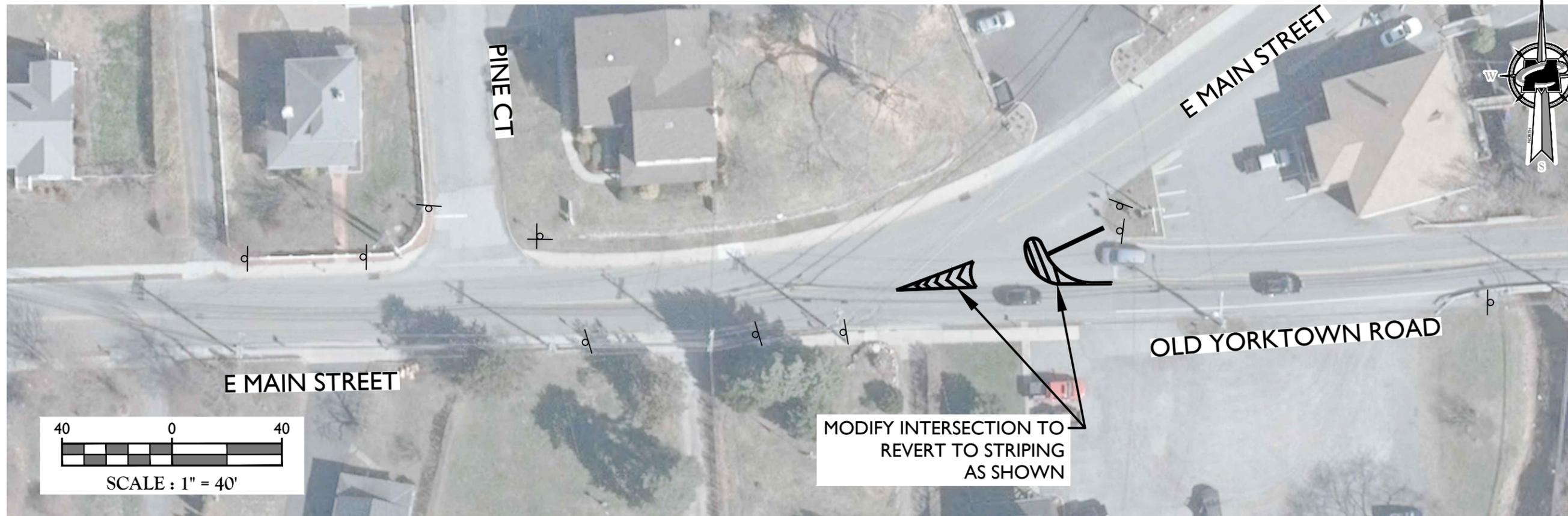
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PROJECT NUMBER: 17005162A	DRAWING NAME: R-TRAF-CALM-CNPT		

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REV	DATE	DESCRIPTION



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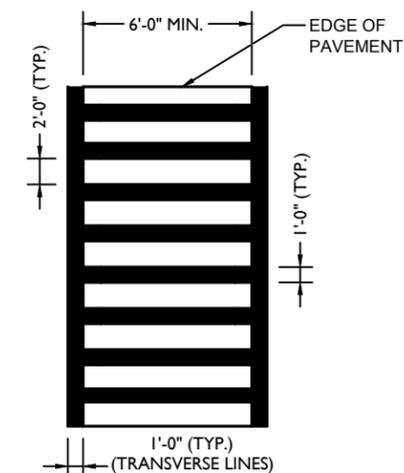
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PROJECT NUMBER:	DRAWING NAME:		
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SHEET TITLE:  
**DETAILS**

SHEET NUMBER:  
**8 of 10**



**TYPICAL CROSSWALK DETAILS**

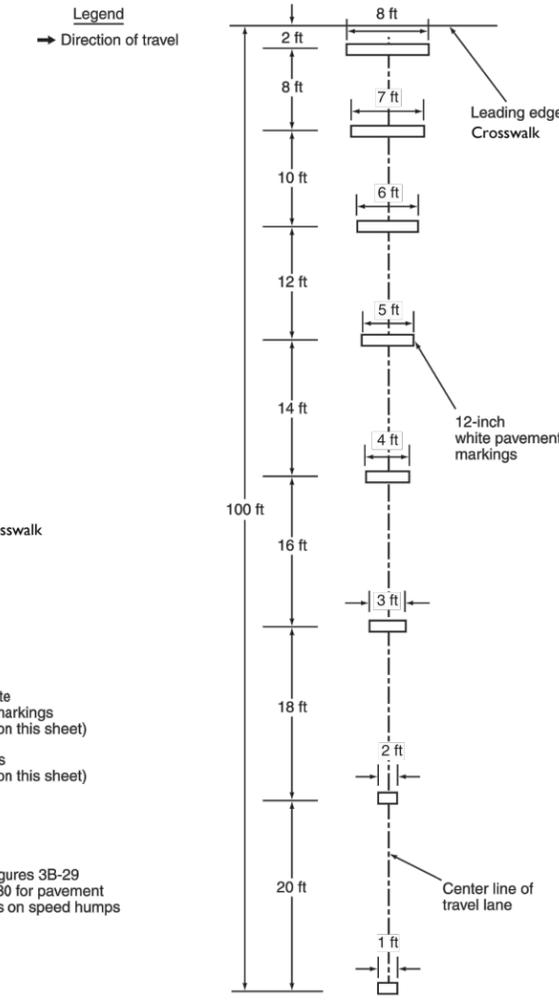
NOTES:  
1. ALL CROSSWALK MARKINGS SHALL BE WHITE.  
2. CROSSWALKS SHALL HAVE THE LONGITUDINAL LINES PARALLEL TO THE LANE LINES.



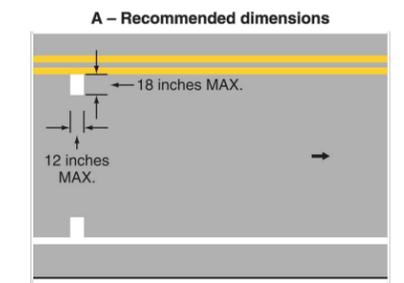
**ADVANCE WARNING SIGN  
(TO BE PLACED 250 FT. IN  
ADVANCE OF CROSSWALKS)**



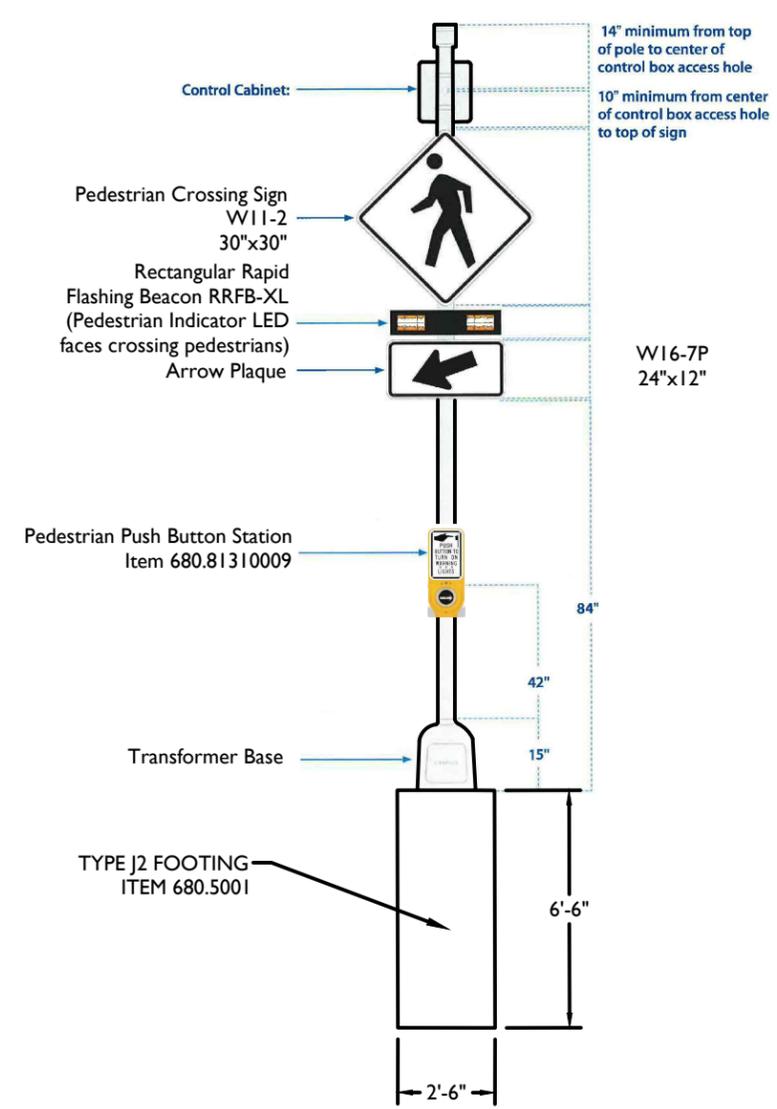
**EVOLIS RADAR SPEED SIGN**



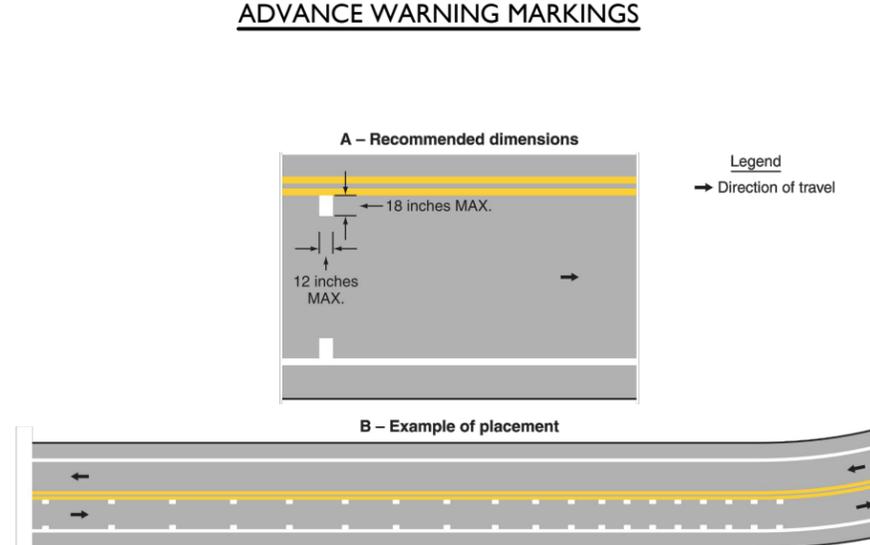
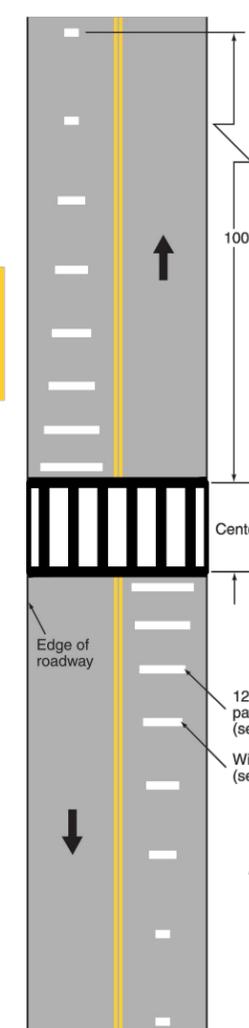
**ADVANCE WARNING MARKINGS**



**SPEED REDUCTION MARKINGS**

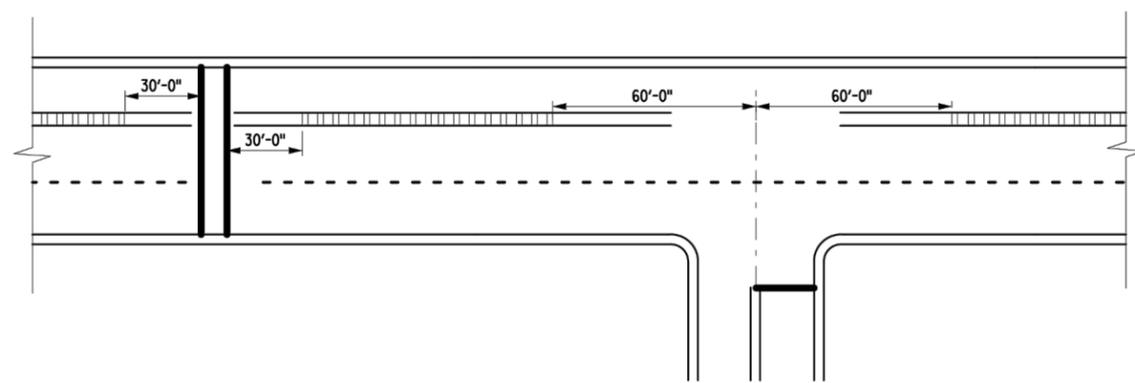


**BI DIRECTION RECTANGULAR RAPID FLASHING BEACON (RRFB)  
ELECTRIC-POWERED WITH BREAKAWAY TRANSFORMER BASE  
(ITEM 680.2240210)  
NOT TO SCALE**



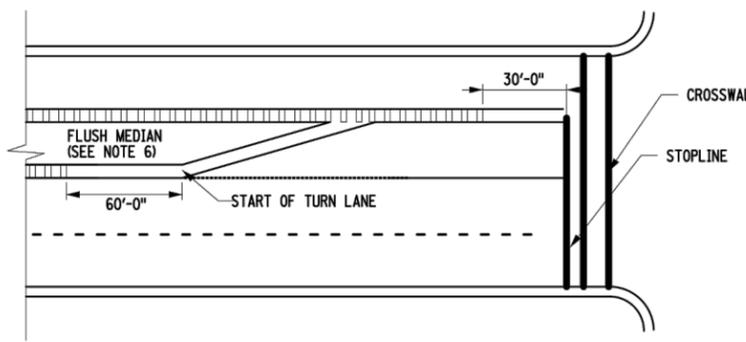
**SPEED REDUCTION MARKINGS**





CARD GAP AT MID BLOCK CROSSWALKS (SEE NOTE 6)

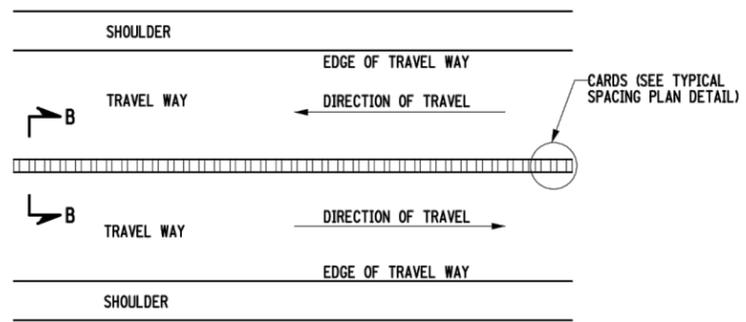
CARD GAPS AT UNCONTROLLED INTERSECTIONS (SEE NOTE 6)



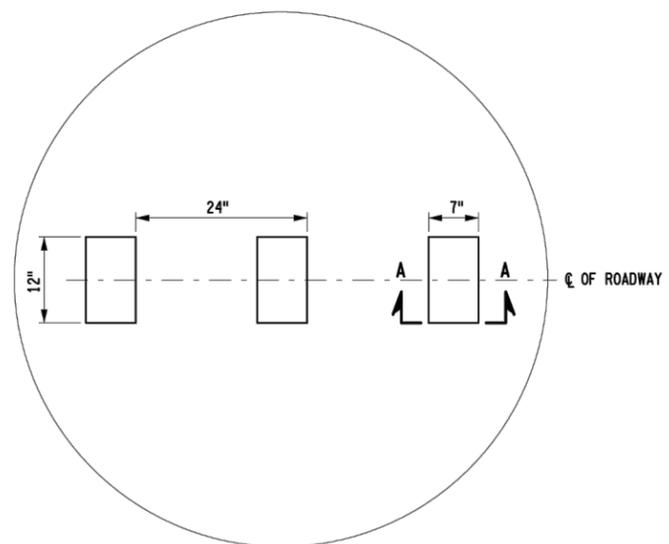
CARD GAPS AT CONTROLLED INTERSECTIONS AND FLUSH MEDIAN (SEE NOTE 6)

NOTES:

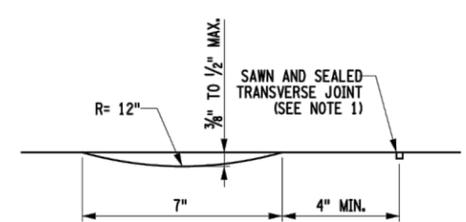
1. NO MILLING SHALL BE DONE WITHIN 4" OF SAWN AND SEALED TRANSVERSE JOINTS. SEE SECTION A-A.
2. ALL ASPHALT MATERIAL MILLED FROM THE CENTERLINE LOCATIONS SHALL BE THOROUGHLY REMOVED FROM THE TRAVELED WAY AND SHOULDERS AND DISPOSED OF IN LOCATIONS APPROVED BY THE ENGINEER.
3. NEW ASPHALT PAVEMENT SHOULD BE ALLOWED TO HARDEN FOR AT LEAST 24 HOURS BEFORE AUDIBLE ROADWAY DELINEATORS ARE MILLED IN.
4. TERMINATION OF CARDS MAY VARY + OR - 5'-0".
5. CARDS SHOULD NOT BE USED WHERE MEDIAN IS A TWO-WAY LEFT TURN LANE.
6. CARDS SHALL BE DISCONTINUED:
  - A. 30' FROM THE STOP BAR OR YIELD LINE AT ANY INTERSECTION ON THE MAINLINE.
  - B. 60' FROM THE CENTERLINE OF ANY INTERSECTING ROADWAY CONTROLLED BY A YIELD SIGN OR STOP SIGN ON THE SIDE STREET.
  - C. 30' FROM ANY CROSSWALKS.
  - D. 30' FROM THE OPENING OF A COMMERCIAL DRIVEWAY THAT SERVES MORE THAN 100 VEHICLES PER DAY AS DETERMINED BY THE ENGINEER.
  - E. 60' FROM LEFT-TURN LANES OR THE BACK OF THE TRAFFIC QUEUE AS DETERMINED BY THE ENGINEER.
  - F. 5' FROM CONCRETE BRIDGE DECKS.



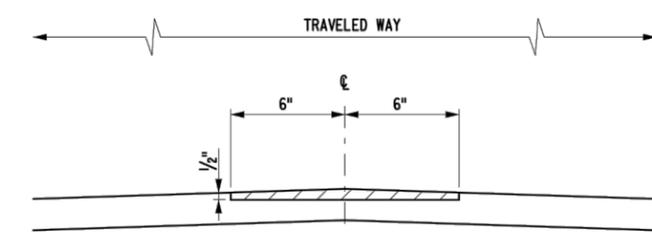
GENERAL PLAN VIEW (CARDS)



TYPICAL SPACING PLAN (CARDS)



SECTION A-A (CARDS)



SECTION B-B (CARDS)

NOTE TO SCALE

EFFECTIVE DATE: 01/09/2014

STATE OF NEW YORK  
DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY STANDARD SHEET

CENTERLINE AUDIBLE ROADWAY DELINEATORS  
( CARDS )

APPROVED 09/06/13 ISSUED UNDER EB 13-040  
/S/ RICHARD W. LEE, P.E. 649-03  
CHIEF ENGINEER (DESIGN)



PHILIP J. GREALY  
NEW YORK LICENSED PROFESSIONAL  
ENGINEER - LICENSE NUMBER: 059858-1

TRAFFIC CALMING  
CONCEPTS  
FOR  
TOWN OF  
YORKTOWN

EAST MAIN STREET  
TOWN OF YORKTOWN  
WESTCHESTER COUNTY  
NEW YORK

WESTCHESTER OFFICE  
400 Columbus Avenue  
Suite 180E  
Valhalla, NY 10595  
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SCALE: AS SHOWN DATE: 4/18/18 DRAWN BY: M.J.A. CHECKED BY: A.P.R./P.J.G.  
PROJECT NUMBER: 17005162A DRAWING NAME: R-TRAF-CALM-CNPT

SHEET TITLE: DETAILS

SHEET NUMBER: 10 of 10