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REV	DATE	DRAWN BY	DESCRIPTION



**PHILIP J. GREALY**  
 NEW YORK LICENSED PROFESSIONAL  
 ENGINEER - LICENSE NUMBER: 059858-1

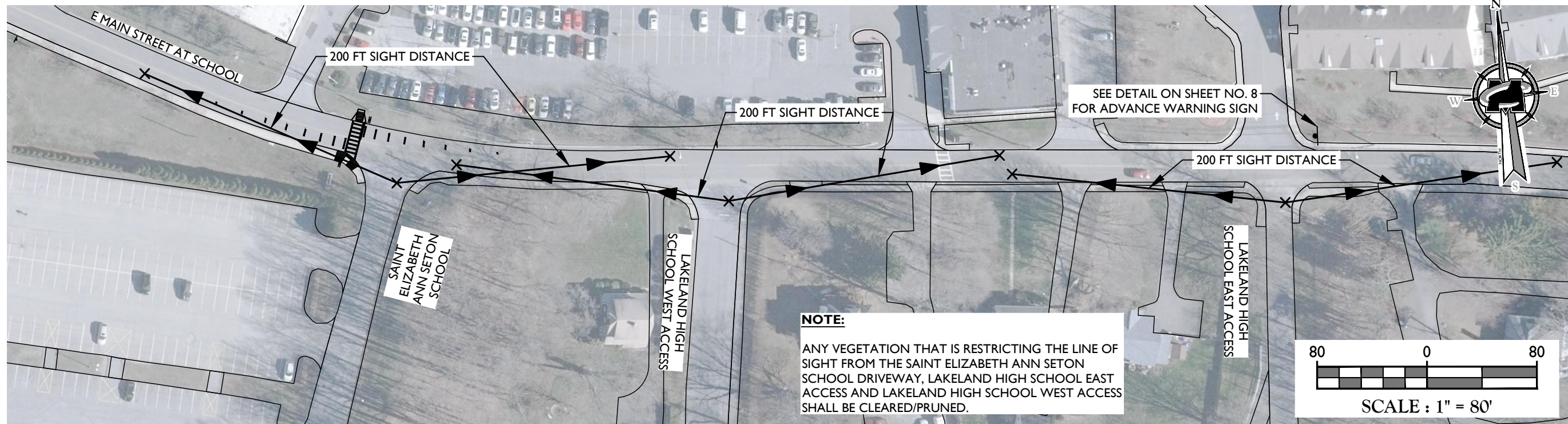
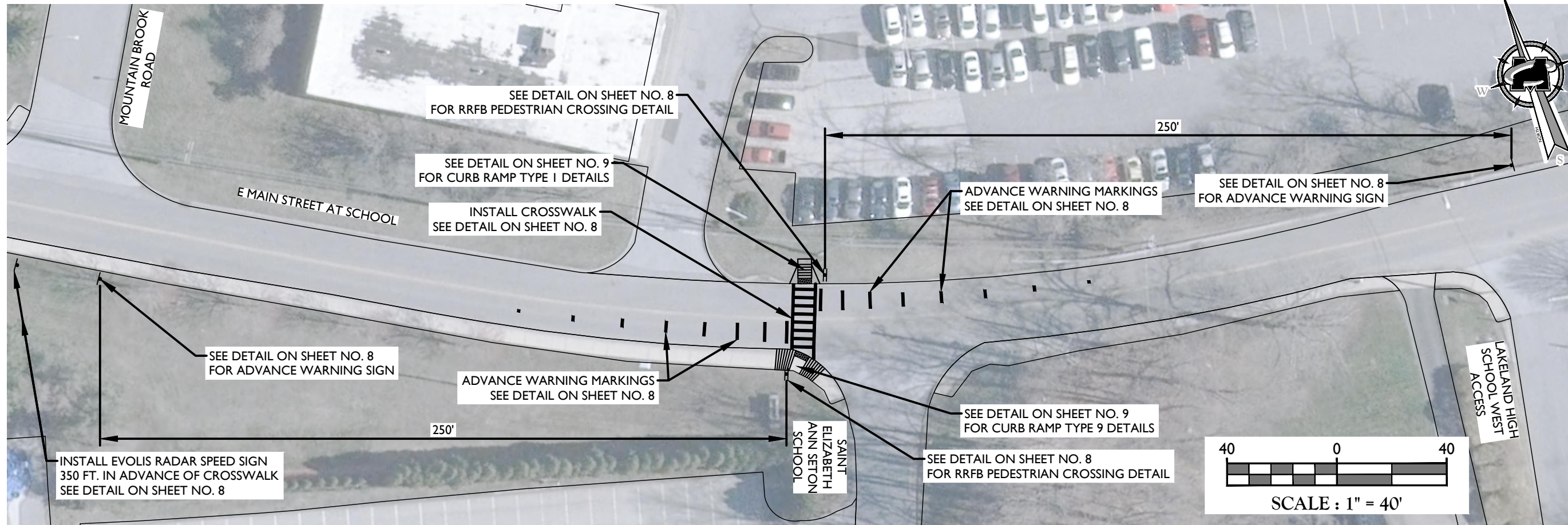
**TRAFFIC CALMING  
 CONCEPTS  
 FOR  
 TOWN OF  
 YORKTOWN**  
 EAST MAIN STREET  
 TOWN OF YORKTOWN  
 WESTCHESTER COUNTY  
 NEW YORK

**WESTCHESTER OFFICE**  
 400 Columbus Avenue  
 Suite 180E  
 Valhalla, NY 10595  
 Phone: 914.347.7500  
 Fax: 914.347.7266

SCALE:	DATE:	DRAWN BY:	CHECKED BY:
AS SHOWN:	4/18/18	M.J.A.	A.P.R./P.J.G.
PROJECT NUMBER:	DRAWING NAME:		
17005162A	R-TRAF-CALM-CNPT		

SHEET TITLE:  
**KEY SHEET**

SHEET NUMBER:  
 I of 10



**NOTE:**  
 ANY VEGETATION THAT IS RESTRICTING THE LINE OF SIGHT FROM THE SAINT ELIZABETH ANN SETON SCHOOL DRIVEWAY, LAKELAND HIGH SCHOOL EAST ACCESS AND LAKELAND HIGH SCHOOL WEST ACCESS SHALL BE CLEARED/PRUNED.

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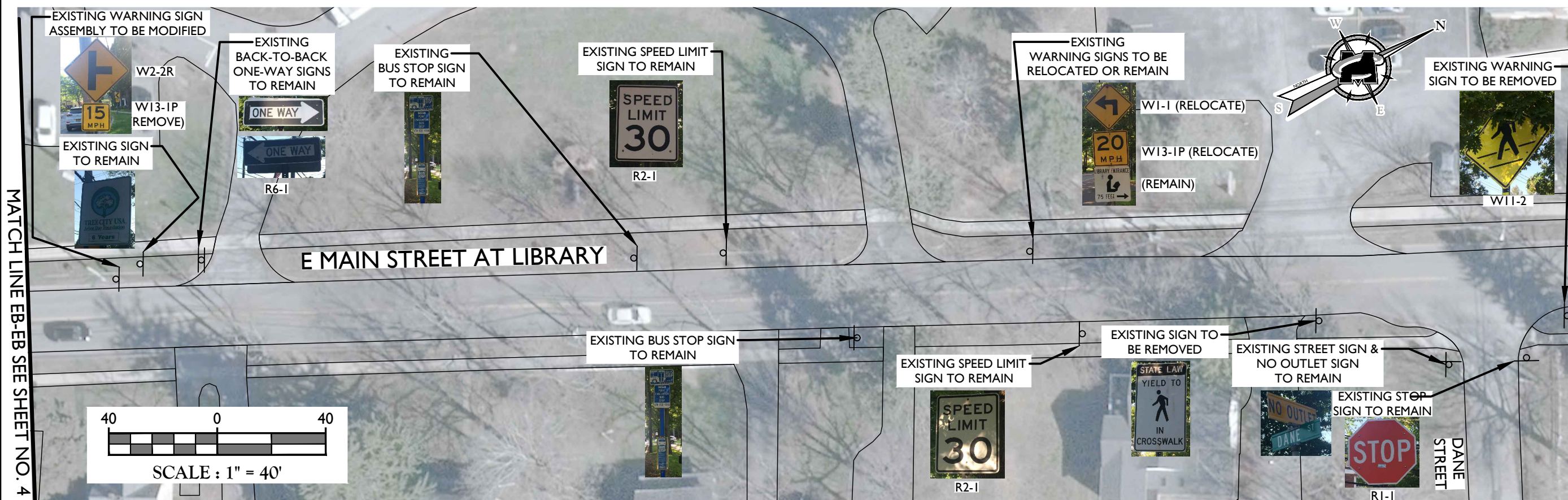
SCALE: AS SHOWN	DATE: 4/18/18	DRAWN BY: M.J.A.	CHECKED BY: A.P.R./P.J.G.
PROJECT NUMBER: 17005162A	DRAWING NAME: R-TRAF-CALM-CNPT		

SHEET TITLE:  
**SUBJECT AREA A**





# EXISTING

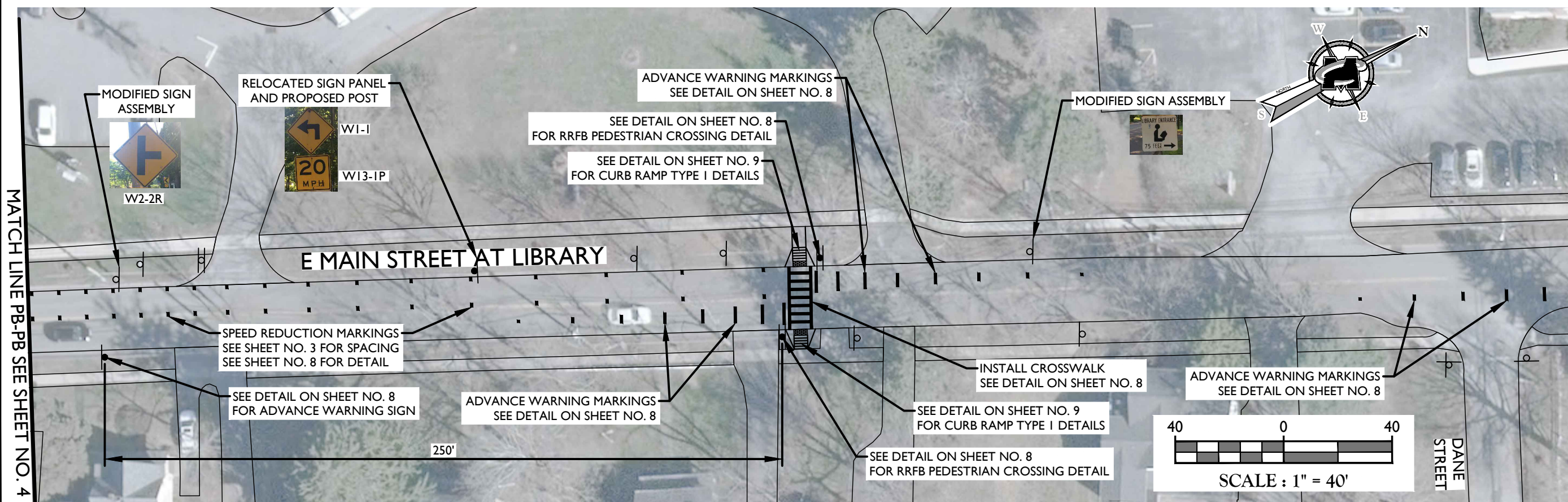


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17005162A	R-TRAF-CALM-CNPT		

SHEET TITLE:  
**SUBJECT AREA B**

SHEET NUMBER:  
 5 of 10

**EXISTING**

MATCH LINE EC-EC SEE SHEET NO. 5



**PROPOSED**

MATCH LINE PC-PC SEE SHEET NO. 5



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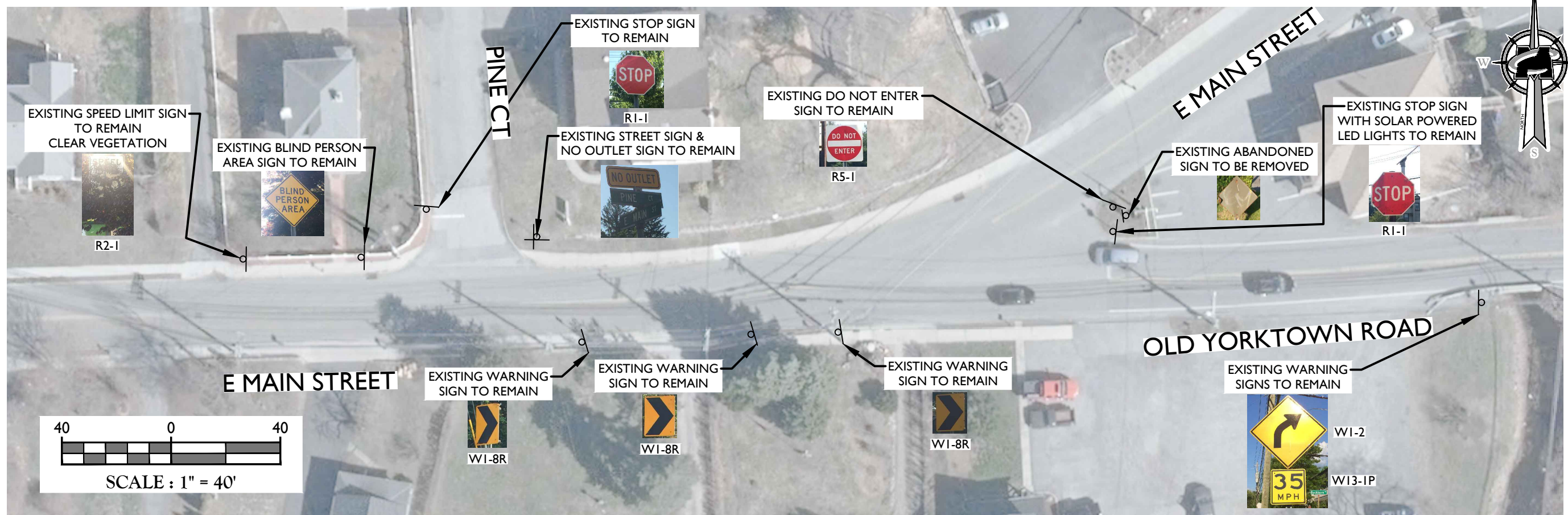
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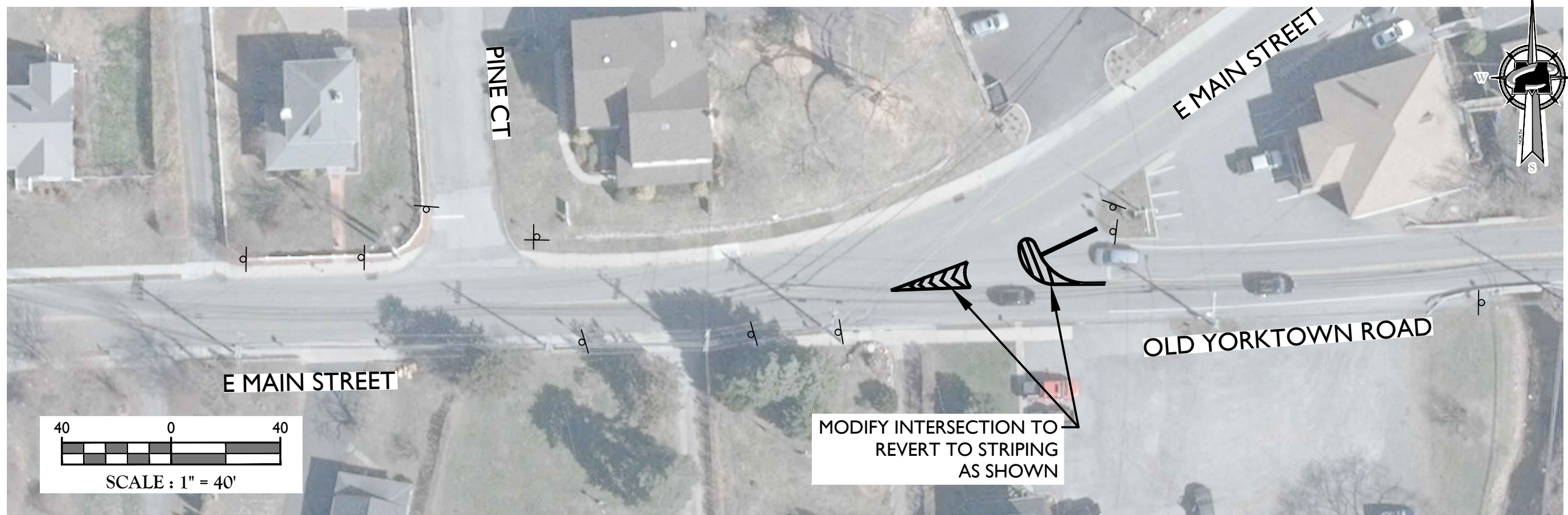
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PROJECT NUMBER: 17005162A	DRAWING NAME: R-TRAF-CALM-CNPT		

SHEET TITLE:  
 SUBJECT AREA B

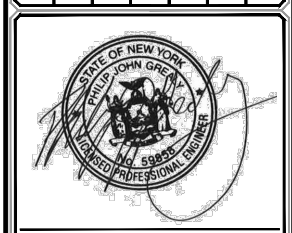
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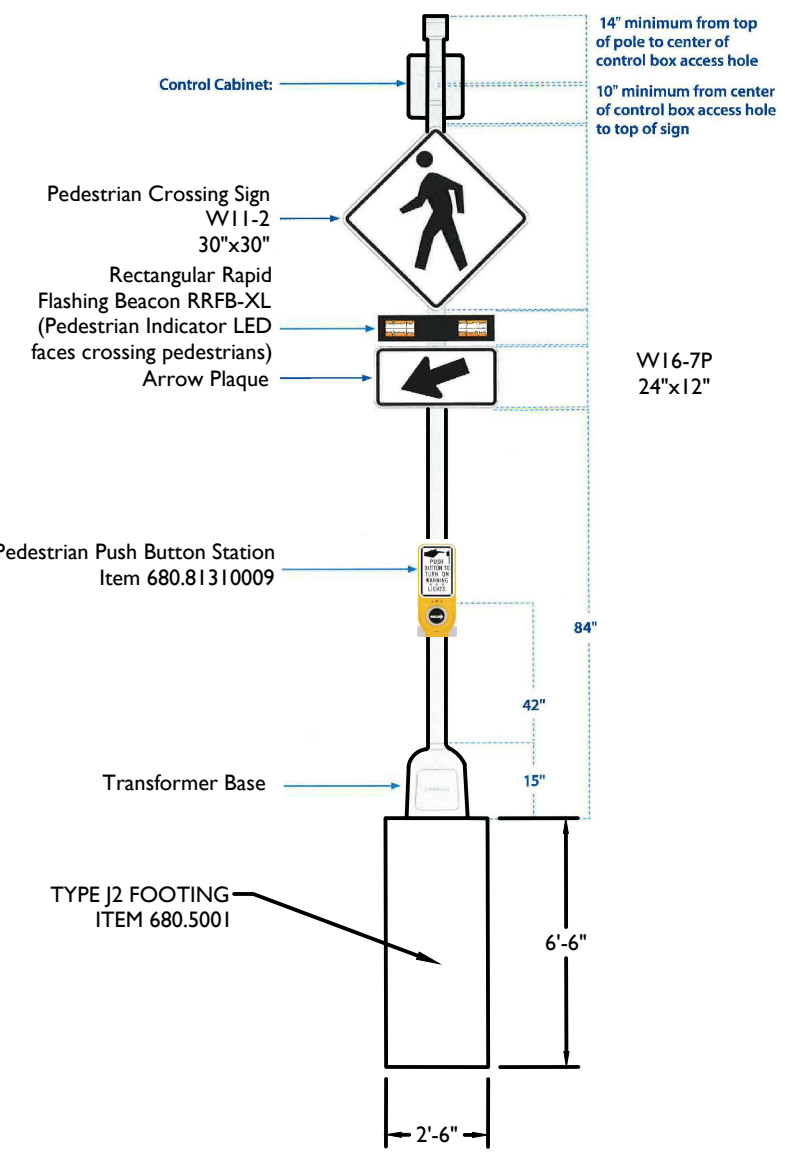
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TRAFFIC CALMING CONCEPTS FOR TOWN OF YORKTOWN  
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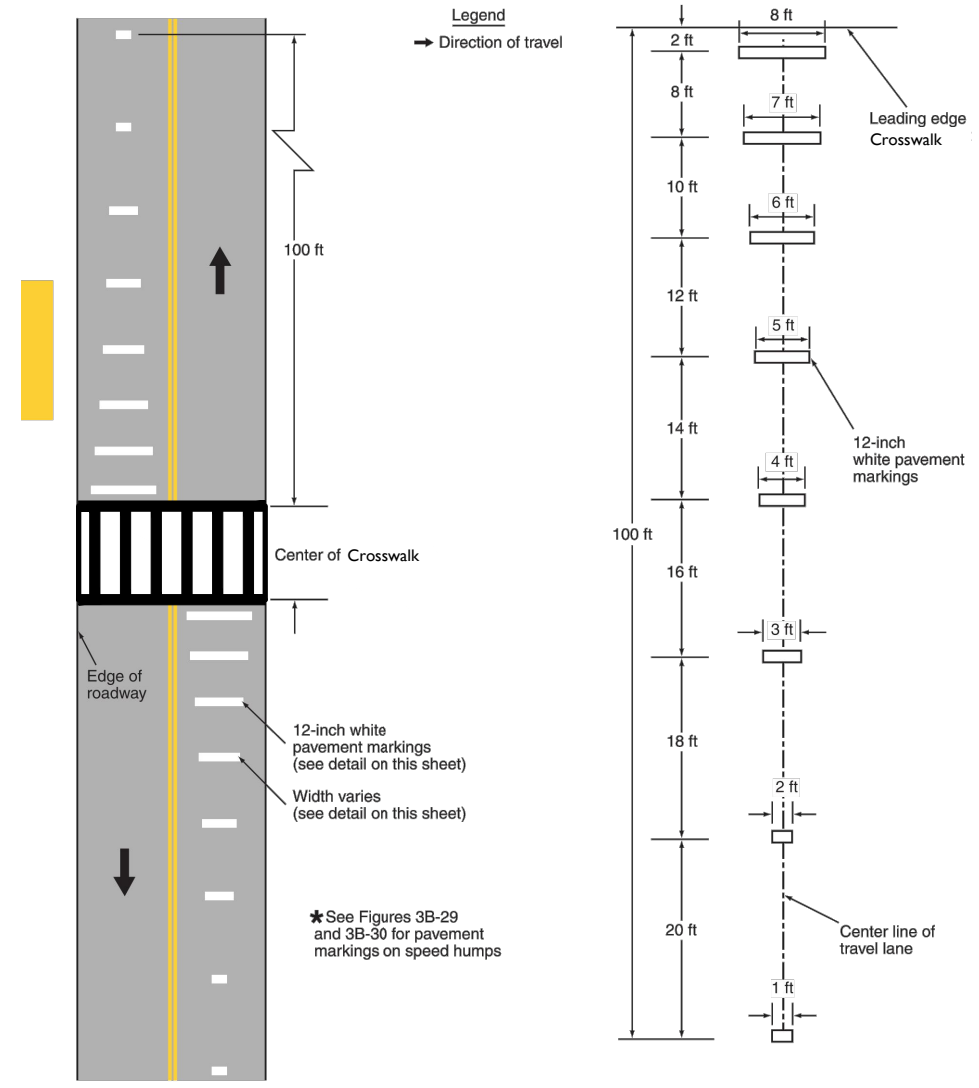
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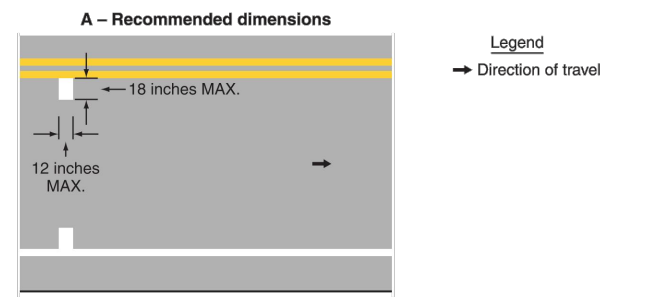
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**SUBJECT AREA C**



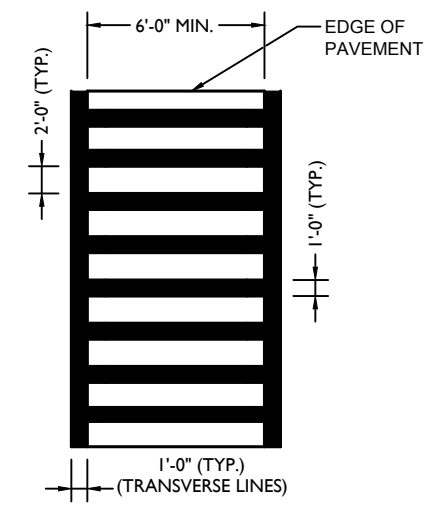
**BI DIRECTION RECTANGULAR RAPID FLASHING BEACON (RRFB)  
 ELECTRIC-POWERED WITH BREAKAWAY TRANSFORMER BASE  
 (ITEM 680.82240210)  
 NOT TO SCALE**



**ADVANCE WARNING MARKINGS**

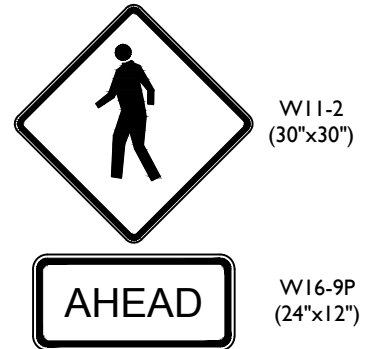


**SPEED REDUCTION MARKINGS**



**COMBINED TYPE LS  
 TYPICAL CROSSWALK DETAILS**

- NOTES:
1. ALL CROSSWALK MARKINGS SHALL BE WHITE.
  2. CROSSWALKS SHALL HAVE THE LONGITUDINAL LINES PARALLEL TO THE LANE LINES.



**ADVANCE WARNING SIGN  
 (TO BE PLACED 250 FT. IN  
 ADVANCE OF CROSSWALKS)**



**EVOLIS RADAR SPEED SIGN**

NO.	DESCRIPTION	DRAWN BY	DATE	REV

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 ENGINEER - LICENSE NUMBER: 059858-1

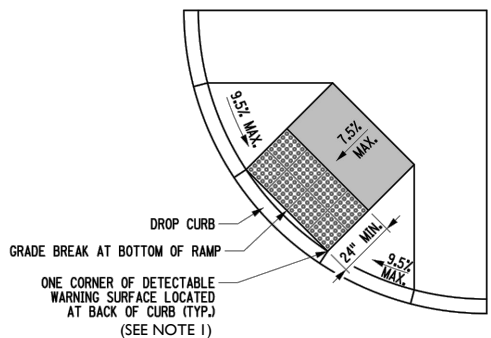
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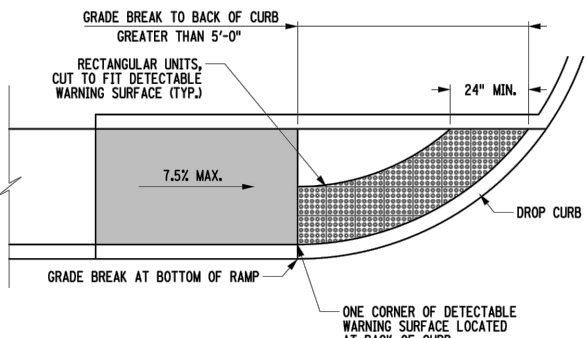
SHEET TITLE:  
**DETAILS**





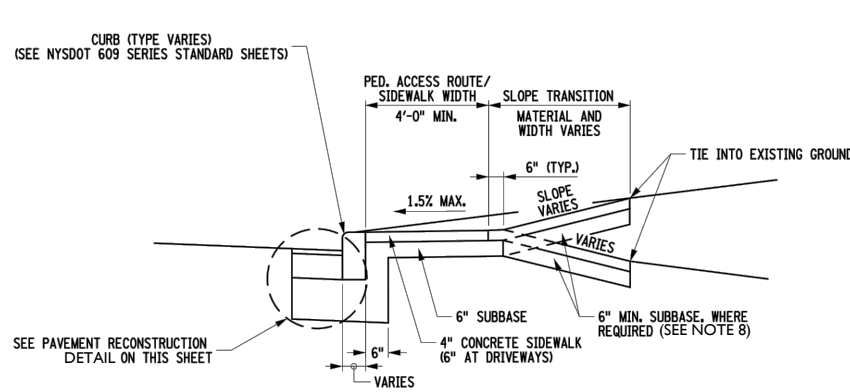
**DETECTABLE WARNING FIELD PLACEMENT OPTION 1**

NOT TO SCALE



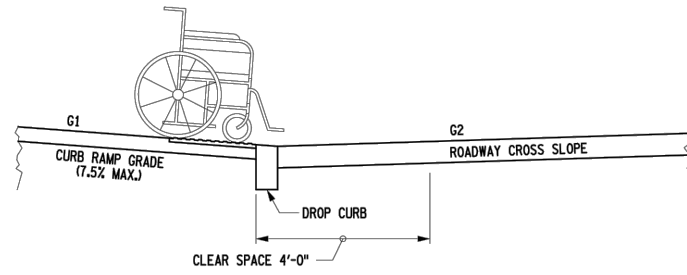
**DETECTABLE WARNING FIELD PLACEMENT OPTION 4**

NOT TO SCALE



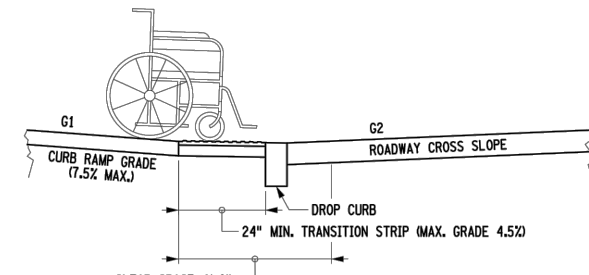
**TYPICAL SIDEWALK CROSS SECTION CURBED WITHOUT BUFFER ZONE**

NOT TO SCALE



**COUNTER SLOPE CONDITION 1**

A = |G2 - G1|  
ALGEBRAIC DIFFERENCE BETWEEN ROADWAY CROSS SLOPE AND CURB RAMP GRADE IS LESS THAN 12.5%.

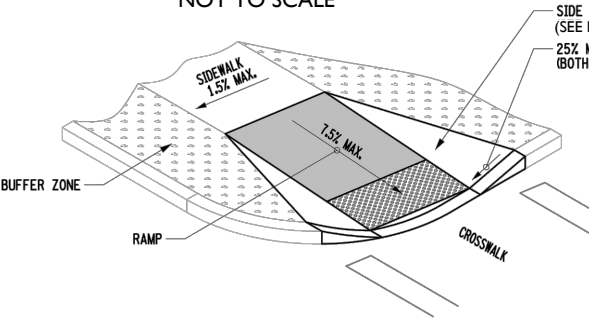


**COUNTER SLOPE CONDITION 2**

A = |G2 - G1|  
ALGEBRAIC DIFFERENCE BETWEEN ROADWAY SLOPE AND CURB RAMP GRADE IS GREATER THAN 12.5%. TRANSITION STRIP REQUIRED (MAX. GRADE 4.5%)

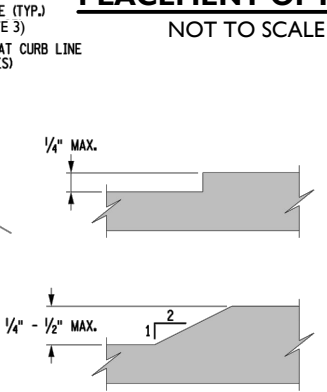
**COUNTER SLOPE CONDITIONS**

NOT TO SCALE



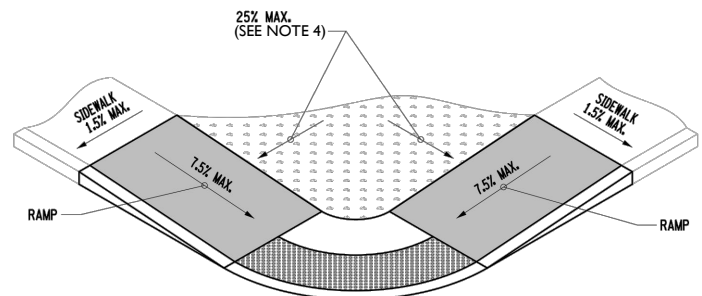
**RAMP SIDE CONFIGURATION OPTION A: FLARED CONCRETE**

NOT TO SCALE



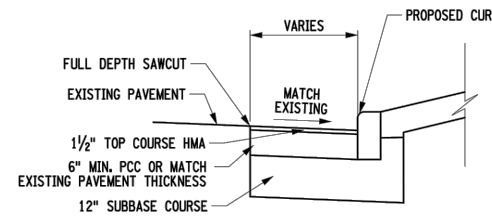
**VERTICAL SURFACE DISCONTINUITIES**

NOT TO SCALE  
(SEE NOTE 2)



**RAMP SIDE CONFIGURATION PARALLEL BACK RAMP TREATMENT OPTION A: GRADED EARTH AND TURF**

NOT TO SCALE

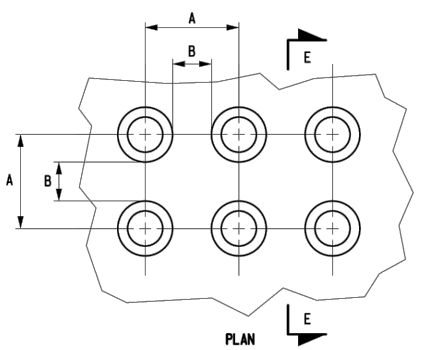


**PAVEMENT RECONSTRUCTION PORTLAND CEMENT CONCRETE**

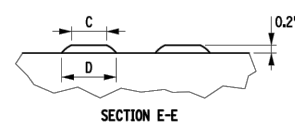
NOT TO SCALE  
(SEE NOTE 9)

**CURB RAMP NOTES:**

- SOME DETECTABLE WARNING PRODUCTS REQUIRE A CONCRETE BORDER FOR PROPER INSTALLATION. IF REQUIRED, THE BORDER SHALL NOT EXCEED 2". WHERE THE BACK OF CURB EDGE IS TOOLED TO PROVIDE A RADIUS, THE BORDER DIMENSION SHALL BE MEASURED FROM THE INSIDE EDGE OF THE CURB RADIUS.
- JOINTS BETWEEN SIDEWALKS, CURB RAMP, TURNING SPACES AND ROADWAYS SHALL BE FLUSH AND FREE FROM ABRUPT VERTICAL CHANGES GREATER THAN 1/4". VERTICAL SURFACE DISCONTINUITIES BETWEEN 1/4" AND 1/2" SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 1:2. THE BEVEL SHALL BE APPLIED ACROSS THE ENTIRE JOINT. SEE DETAIL ON THIS SHEET.
- RAMP SIDE OPTIONS ARE DETAILED ON THIS SHEET FOR USE WITHIN THE BUFFER ZONE. WHERE A PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP, FLARED SIDES SHALL BE INSTALLED WITH A MAXIMUM SLOPE OF 9.5% FOR DESIGN AND LAYOUT, AND 10% MAXIMUM FOR WORK ACCEPTANCE. THE SLOPE OF FLARED SIDES IS MEASURED PARALLEL TO THE CURB LINE.
- THE BACKSIDE OF A PARALLEL RAMP SHOULD BE GRADED TO A MAXIMUM SLOPE OF 25% TO MATCH EXISTING TERRAIN, UNLESS OTHERWISE SHOWN IN THE CONTRACT DOCUMENTS WHERE GRADING IS NOT FEASIBLE DUE TO LIMITED ROW OR PHYSICAL CONSTRAINTS, A BACK CURB MAY BE INSTALLED. SEE DETAILS ON THIS SHEET.
- BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE OF 4' - 0" X 4' - 0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK, AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. THE CLEAR SPACE MAY OVERLAP TURNING SPACES, DETECTABLE WARNING SURFACES, AD DROP CURBS. REFERENCE SHALL BE MADE TO NYSDOT STANDARD SHEET 608-01.
- WHERE A CHANGE IN DIRECTION IS REQUIRED TO UTILIZE A CURB RAMP, A TURNING SPACE SHALL BE PROVIDED AT THE BASE OR THE TOP OF CURB RAMP AS APPLICABLE. TURNING SPACES SHALL BE PERMITTED TO OVERLAP CLEAR SPACES.
- TURNING SPACES SHALL NOT BE DESIGNED WITH CROSS SLOPE GREATER THAN 1.5% IN ANY DIRECTION, WHILE PROVIDING POSITIVE DRAINAGE. THE MAXIMUM CROSS SLOPE FOR WORK ACCEPTANCE IS 2.0%. A NONSTANDARD FEATURE JUSTIFICATION IS REQUIRED WHERE TURNING SPACES EXCEED 2.0% IN ANY DIRECTION.
- MATERIAL DEPTHS SHOWN ON THESE SHEETS ARE TYPICAL MINIMUM VALUES AND MAY BE DIFFERENT IN THE CONTRACT DOCUMENTS.
- WHERE EXISTING ROADWAYS ARE SAWCUT TO INSTALL CURBING AND/OR SIDEWALK, THE ROADWAY SHOULD BE SAWCUT AT LEAST 2'-0" FROM THE PROPOSED CURB LINE TO ALLOW FOR ADEQUATE COMPACTION OF ASPHALT. IF SAWCUT IS LESS THAN 2'-0" FROM PROPOSED CURB LINE, THEN THE ROADWAY SHALL BE REBUILT USING CLASS C CONCRETE. SEE DETAILS ON THIS SHEET.

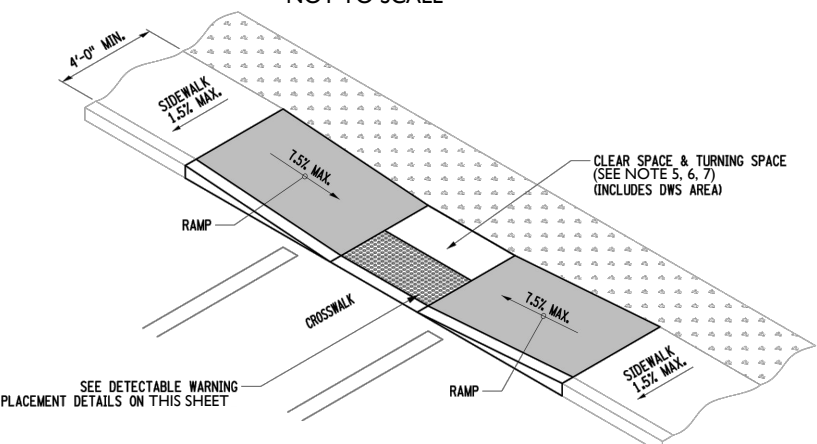


TRUNCATED DOME DIMENSIONS		
DIM.	MIN. (IN)	MAX. (IN)
A	1.6"	2.4"
B	0.65"	1.5"
C	50% - 65% OF D DIM.	
D	0.9"	1.4"



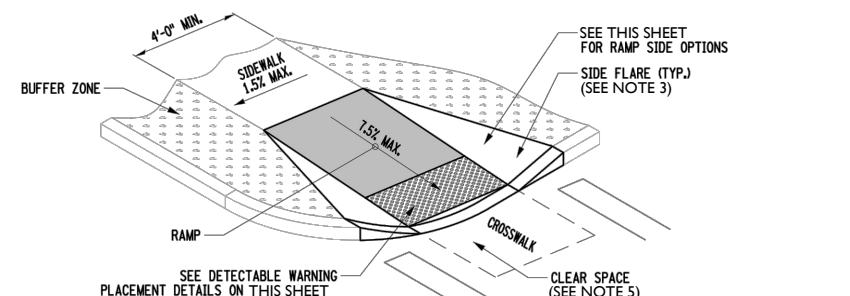
**DETECTABLE WARNING FIELD DOME DETAIL**

NOT TO SCALE



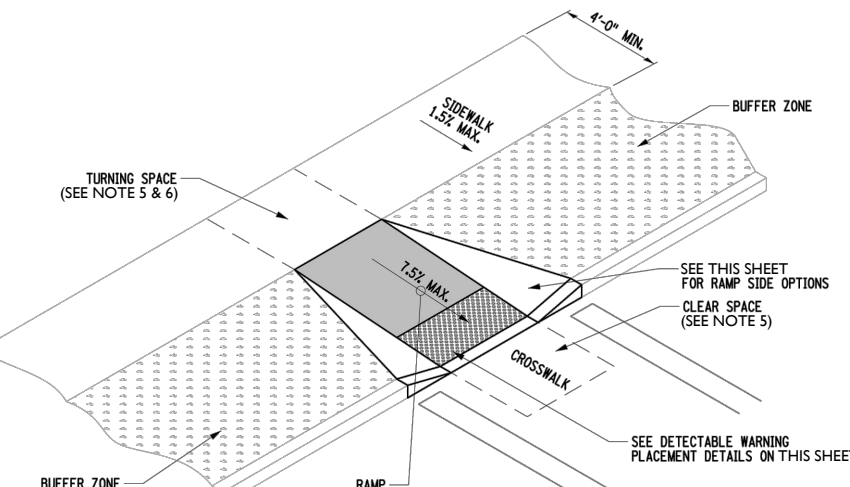
**CURB RAMP CONFIGURATION TYPE 9 MID BLOCK CROSSING OR T INTERSECTION**

NOT TO SCALE



**CURB RAMP CONFIGURATION TYPE I**

NOT TO SCALE



**CURB RAMP CONFIGURATION TYPE II MID BLOCK CROSSING OR T INTERSECTION**

NOT TO SCALE

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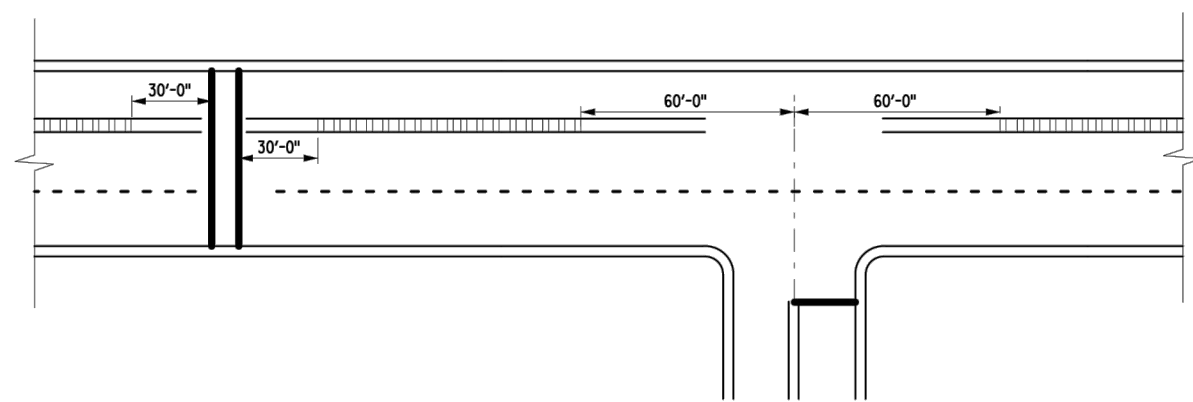
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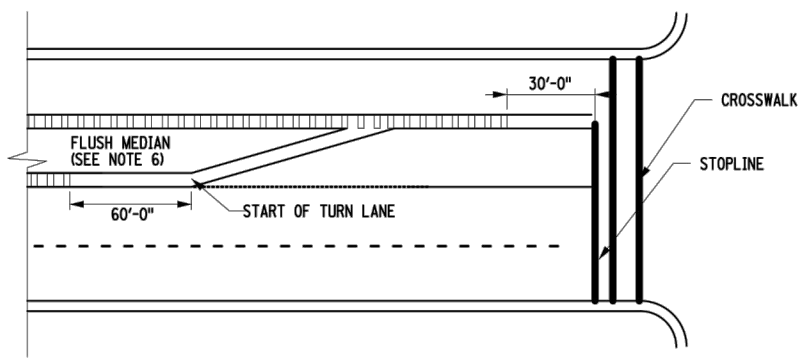
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SHEET TITLE: DETAILS  
SHEET NUMBER: 9 of 10



CARD GAP AT MID BLOCK CROSSWALKS (SEE NOTE 6)

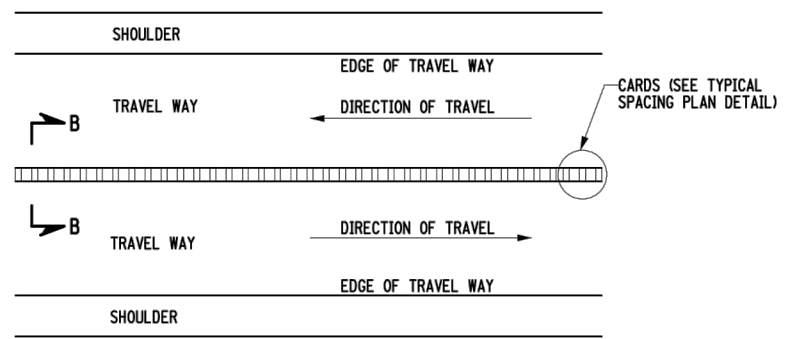
CARD GAPS AT UNCONTROLLED INTERSECTIONS (SEE NOTE 6)



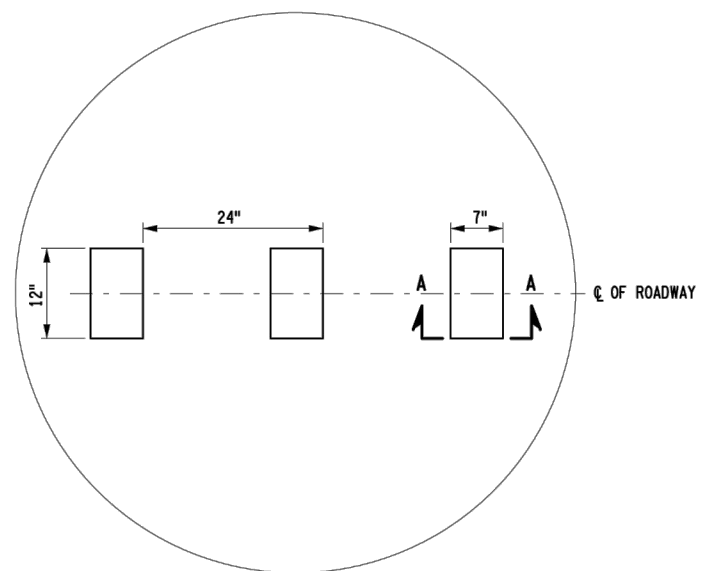
CARD GAPS AT CONTROLLED INTERSECTIONS AND FLUSH MEDIAN (SEE NOTE 6)

NOTES:

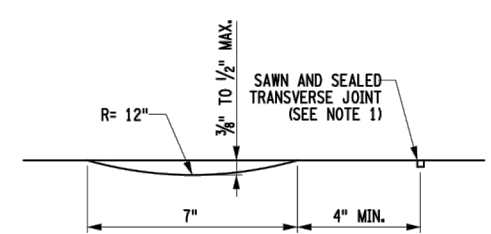
- NO MILLING SHALL BE DONE WITHIN 4" OF SAWN AND SEALED TRANSVERSE JOINTS. SEE SECTION A-A.
- ALL ASPHALT MATERIAL MILLED FROM THE CENTERLINE LOCATIONS SHALL BE THOROUGHLY REMOVED FROM THE TRAVELED WAY AND SHOULDERS AND DISPOSED OF IN LOCATIONS APPROVED BY THE ENGINEER.
- NEW ASPHALT PAVEMENT SHOULD BE ALLOWED TO HARDEN FOR AT LEAST 24 HOURS BEFORE AUDIBLE ROADWAY DELINEATORS ARE MILLED IN.
- TERMINATION OF CARDS MAY VARY + OR - 5'-0".
- CARDS SHOULD NOT BE USED WHERE MEDIAN IS A TWO-WAY LEFT TURN LANE.
- CARDS SHALL BE DISCONTINUED:
  - 30' FROM THE STOP BAR OR YIELD LINE AT ANY INTERSECTION ON THE MAINLINE.
  - 60' FROM THE CENTERLINE OF ANY INTERSECTING ROADWAY CONTROLLED BY A YIELD SIGN OR STOP SIGN ON THE SIDE STREET.
  - 30' FROM ANY CROSSWALKS.
  - 30' FROM THE OPENING OF A COMMERCIAL DRIVEWAY THAT SERVES MORE THAN 100 VEHICLES PER DAY AS DETERMINED BY THE ENGINEER.
  - 60' FROM LEFT-TURN LANES OR THE BACK OF THE TRAFFIC QUEUE AS DETERMINED BY THE ENGINEER.
  - 5' FROM CONCRETE BRIDGE DECKS.



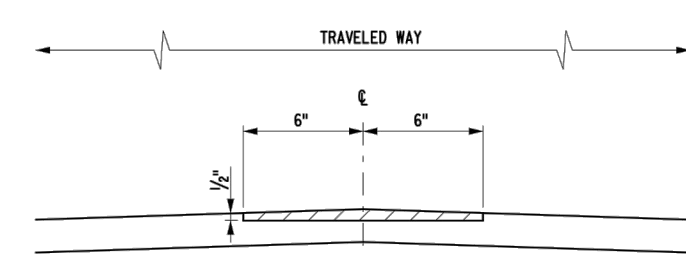
GENERAL PLAN VIEW (CARDS)



TYPICAL SPACING PLAN (CARDS)



SECTION A-A (CARDS)



SECTION B-B (CARDS)

REV	DATE	DRAWN BY	DESCRIPTION



**PHILIP J. GREALY**  
NEW YORK LICENSED PROFESSIONAL ENGINEER - LICENSE NUMBER: 059858-1

TRAFFIC CALMING CONCEPTS FOR TOWN OF YORKTOWN

EAST MAIN STREET  
TOWN OF YORKTOWN  
WESTCHESTER COUNTY  
NEW YORK

STATE OF NEW YORK  
DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY STANDARD SHEET

CENTERLINE AUDIBLE ROADWAY DELINEATORS (CARDS)

APPROVED 09/06/13 ISSUED UNDER EB 13-040  
/S/ RICHARD W. LEE, P.E. CHIEF ENGINEER (DESIGN)

649-03

NOTE TO SCALE  
EFFECTIVE DATE: 01/09/2014

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SCALE: AS SHOWN	DATE: 4/18/18	DRAWN BY: M.J.A.	CHECKED BY: A.P.R./P.J.G.
PROJECT NUMBER: 17005162A	DRAWING NAME: R-TRAF-CALM-CNPT		

SHEET TITLE: DETAILS

SHEET NUMBER: 10 of 10